

Rezoning Transportation Analysis

Petition Number: #2022-162

General Location Identifier: 23109479

From: Jake Carpenter, PE

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Reviewer:

Isaiah Washington

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Revision Log:

Date	Description
09-29-22	First Review (PDM)
10-26-22	Second Review (IW)
11-15-22	Third Review (DR)
11-30-22	Fourth Review (DR)

General Review Information

The petition is located on the west side of Weddington Road, a State-maintained major throughfare, south of Simfield Church Road, a State-maintained minor collector. The petition is located in a south wedge outside of Route 4, and within the I-485 Interchange Analysis Study Area.

Active Projects Near the Site:

- N/A

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The petition is located on the west side of Weddington Road, a State-maintained major throughfare south of Simfield Church Road, a State-maintained minor collector. A Traffic Impact Study (TIS) is not needed for this. The petitioner has committed to installing turn lanes at the site access, constructing a 12-foot Multi-Use Path on the south side of the site access, and dimensioning the Right-of-Way. Site plan revisions are needed to meet ordinance requirements and/or the outstanding items including, but not limited to, labeling and dimensioning the curb and gutter from the centerline of Weddington Road, illustrating turn lane improvements, and depicting, labeling, and dimensioning the proposed multi-use path. Further details are listed below.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Vacant	-	-	<i>Tax Record</i>
Entitlement with Current Zoning	Multifamily (R-12MF, 7.23 acres)	70 Dwelling Units	290	<i>General Guidance from Planning & RZP # 2013-095</i>
Proposed Zoning	Multifamily (Mid-Rise) (R-17MF, 7.23 acres)	90 Dwelling Units	385	Site Plan: 08-19-22
Proposed Zoning	Multifamily (Mid-Rise) (R-17MF, 7.23 acres)	96 Dwelling Units	415	Site Plan: 10-14-22

Provide comments to the specified comments below.

Outstanding Issues

Strikethrough = Resolved

1. **Curbline:**

Outstanding Comment from 09-29-2022

- a. **Weddington Road:** The future location of the back of curb and gutter is located 30-feet from the centerline of the roadway.

Label and dimension the future curb and gutter from the centerline for each road on the site plan.

2. **Traffic Study:**

~~A Traffic Impact Study (TIS) is not necessary for the complete review of this petition due to the site generating less than 2,500 daily trips. If during the permitting process the site generates more than 2,500 daily trips, then a traffic study will be required.~~

- 3. ~~Revise site plan to label and dimension the right-of-way from the road centerline on Weddington Road.~~
- 4. Revise site plan and conditional note(s) to commit to construct an 8-foot planting strip, and 12-foot sidewalk on along the entire property frontage of Weddington Road. The site plan shall label and dimension both items from the back of curb and gutter and road centerline.

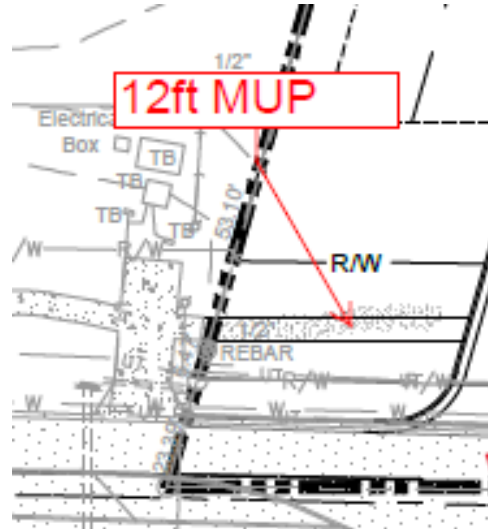
Clarifying Comment 10-27-2022: Illustrate 12 foot path on the site plan along entirety of Weddington Road frontage. Planting strip and multi-use path should be located and dimensioned from the **future** back of curb.

Clarifying Comment 11-30-2022: Planting strip along frontage of Weddington Road should be shown as 8 feet. Any changes to location of streetscape elements necessitated because of the right turn lane should be located and dimensioned from the **future** back of curb.

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5. Per coordination with NCDOT, left and right turn lanes are required at the proposed site access. Site plan and conditional note(s) revisions are needed to provide a right turn lane, and to restripe the existing median to a left turn.

Clarifying Comment 10-27-22: Depict left and right turn lane improvements on site plan and include any changes to location of streetscape elements necessitated because of the right turn lane.

- ~~6. A site plan note(s) specifying dedication and fee simple conveyance of all rights-of-way to the City before the site's first building certificate of occupancy is issued as needed. CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible.~~
- ~~7. Add conditional note "All public roadway improvements will be subject to the standards and criteria of CDOT and NCDOT, as applicable, to the roadway improvements within their respective road system authority. It is understood that such improvements may be undertaken by the Petitioner on its own or in conjunction with other development or roadway projects taking place within the broad southeastern Mecklenburg area, by way of a private/public partnership effort or other public sector project support."~~

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align

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with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.

4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.
7. The petitioner needs to complete and submit the [Right of way Abandonment Petition form](#) to CDOT for review. The decision of this rezoning petition does not correlate with the decision of the possible abandonment, as this decision is issued within the separate Right of Way Abandonment process that is controlled by North Carolina General Statutes.
8. To attain a street and pedestrian lighting recommendation, the petitioner should coordinate with Anthony Mendez, CDOT. Please visit the following link:
<https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>