

Rezoning Transportation Analysis

Petition Number: 2022-118

General Location Identifier: 10810101, 10810105, 10810122, 10810124

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Revision Log:	Date	Description
	12-29-22	First Review (IW)
	01-26-23	Second Review (IW)

General Review Information

The petition is located on the west side of Harrisburg Road, a State-maintained major throughfare north of Ponderosa Pine Lane, a City maintained local street. The petition is located in an east wedge outside of Route 4, and within the Eastside Strategy Plan Study Area.

Active Projects Near the Site:

- There are no active projects in the vicinity of project

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The petition is located on the west side of Harrisburg Road, a State-maintained major throughfare north of Ponderosa Pine Lane, a City maintained local street. Traffic Impact Study (TIS) is not required for this site. Site plan revisions are needed to meet ordinance requirements and/or the outstanding items including, but not limited to committing to and 8 foot planting strip and 6 foot sidewalk per Chapter 20, installing a left turn lane on Cedarbrook Drive, commit to extending stem length of Cedarbrook Drive Extension and realigning Cedarbrook Drive Extension better with the existing Cedarbrook Drive. Further details are listed below.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Single Family	2 Dwelling Units	20	Tax Record
Entitlement with Current Zoning	Single Family (R-3, 11.841 acres)	35 Dwelling Units	385	General Guidance from Planning
Proposed Zoning	TBD (MX-1 & B-2, 11.841 acres)	TBD	TBD	Site Plan: 06-17-22
Proposed Zoning	Single Family Detached Single Family Attached Retail (MX-1 & B-2, 11.841 acres)	37 Dwelling Units 35 Dwelling Units 10,000 SF	1,275	Site Plan: 01-16-23

Petitioner to provide responses to the staff comments below as well as specify other changes to the site plan.

Outstanding Issues

Strikethrough = Resolved

1. Curblines:

- a. **Harrisburg Road:** Location of future back of curb and gutter to be located to 30 feet from roadway centerline to accommodate the Charlotte Streets Map with a roadway typical section of CLDSM 11.09 Major Thoroughfare.

NOTE: Additional distance from center line to back of curb may be required in locations where a median is present or around intersections and interchanges.

Label and dimension the curb and gutter from the centerline for each road on the site plan.

2. Traffic Study:

~~A Traffic Impact Study (TIS) is not required for the complete review of this petition due to the site generating less than 2,500 daily trips and/or not triggering any other City TIS requirements. (For Conventional Petitions) A Traffic Study may be required during permitting per the TOD or other applicable zoning ordinance.~~

- 3. Revise site plan and conditional note(s) to commit to dedicate 52 feet of right-of-way from the road centerline. The site plan shall label and dimension the right-of-way from the road centerline.
- 4. Per Chapter 20 revise site plan and conditional note(s) to commit to construct an 8-foot planting strip, and 6-foot sidewalk on all internal public roads or if the proposed dwelling units exceeds 12 per acre after verifying density, revise site plan and conditional note(s) to commit to construct an 8-foot planting strip, and 8-foot sidewalk along all internal public Roads. The wider sidewalk also meets the Charlotte WALKS Policy.

The site plan shall label and dimension both items from the back of curb and gutter or edge of pavement.

Reference (CLDSM standard detail U-04.16) for street typical for public roads.

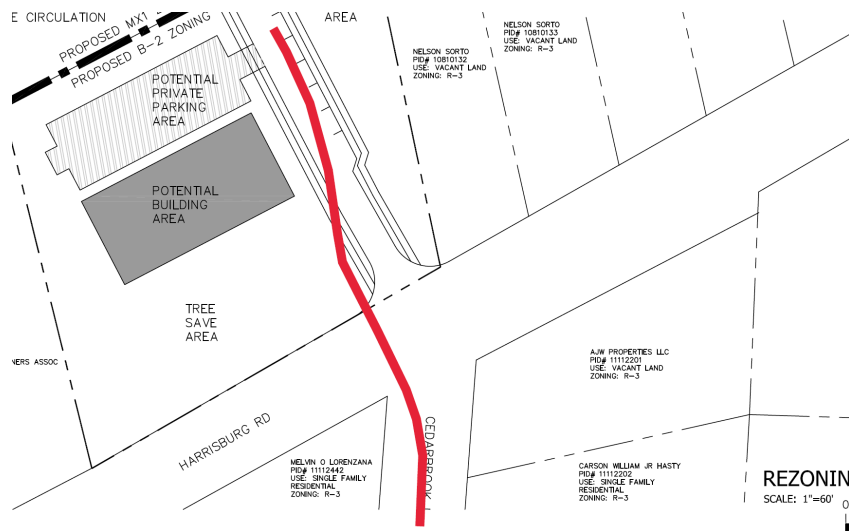
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Clarifying comment 01-26-23: After receiving updated density, the site plan and conditional note(s) should be revised to commit to construct an 8-foot planting strip, and 6-foot sidewalk on all internal public roads per the Chapter 20 ordinance.

5. Revise site plan and conditional note(s) to commit to construct bicycle facilities by installing a 12' multi-use path to meet the Council-adopted Charlotte BIKES Policy along Harrisburg Road. Multi-use paths maintained by the City shall meet ADA/PROWAG requirements.
6. Add a conditional note specifying "A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements."
7. Revise site plan and conditional note(s) to commit to installing a left turn lane for access off of Harrisburg Road per coordination with NCDOT.
8. Revise site plan and conditional note(s) to commit to extending stem length of Cedarbrook Drive Extension to 100 feet, no parking within that stem. Per coordination with NCDOT.
9. Revise site plan and conditional note(s) to commit to acquiring right of way prior to permit approval. Per coordination with NCDOT.
10. CDOT Request: If possible, revise site plan and conditional note(s) to commit to improving Cedarbrook Drive alignment per coordination with NCDOT.

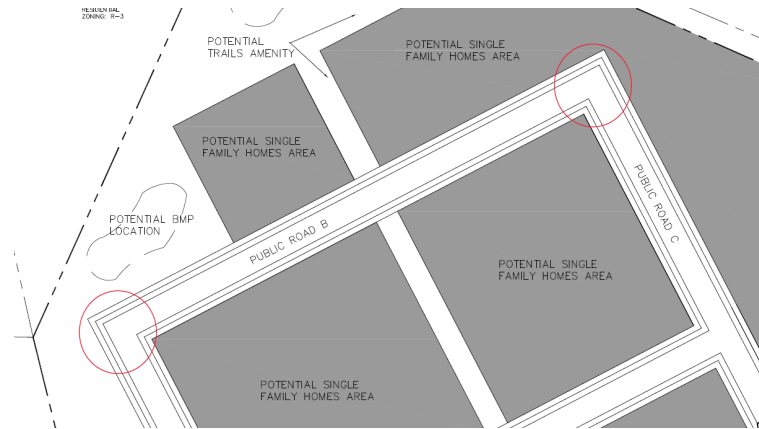


11. Revise design of public roads to remove 90 degree turns.

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12. ~~Add a conditional note specifying “All transportation improvements will be approved and constructed before the site’s first building certificate of occupancy is issued. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes.”~~
13. Add conditional note specifying “All public roadway improvements will be subject to the standards and criteria of CDOT and NCDOT, as applicable, to the roadway improvements within their respective road system authority. It is understood that such improvements may be undertaken by the Petitioner on its own or in conjunction with other development or roadway projects taking place within the broad South Mecklenburg area, by way of a private/public partnership effort or other public sector project support.”

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. To obtain a street and pedestrian lighting recommendation, the petitioner should visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>