

# Rezoning Transportation Analysis

Petition Number: #2022-057

General Location Identifier: 03507103, 03507104, 03507108, 03507109, 03507110

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**Reviewer:**

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## Revision Log:

Date	Description
09-29-22	First Review (PDM)

## General Review Information

The petition is located on the south side of Old Plank Road, a City-maintained minor throughfare, near Almora Drive, a City-maintained local street. The petition is located in the northwest corridor wedge outside of Route 4. The petition is also located within the Westside Strategy Plan Study Area.

Active Projects Near the Site:

- o N/A

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*CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.*

*This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.*

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Based on our review of the petition, we offer the following information for your consideration.

## Transportation Summary

The petition is located on the south side of Old Plank Road, a City-maintained minor throughfare, near Almora Drive, a City-maintained local street. A Traffic Impact Study (TIS) is not required for this site. Site plan revisions are needed to meet ordinance requirements and/or the outstanding items including, but not limited to dedicating 35-feet of Right-of-Way on Old Plank Road, dimensioning the location of the curb and gutter, and committing to the installation of sidewalk, bike lane, and left turn lanes on Old Plank Road. Further details are listed below.

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## Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Single Family Detached	2 Dwelling Units	20	Tax Record
Entitlement with Current Zoning	Single Family Industrial (R-4 & I-1, 21.497 acres)	76 Dwelling Units 24,400 SF	865	General Guidance from Planning
Proposed Zoning	Single Family Attached (R-8MF, 21.497 acres)	100 Dwelling Units	715	Site Plan: 03-28-22

Provide comments to the specified comments below.

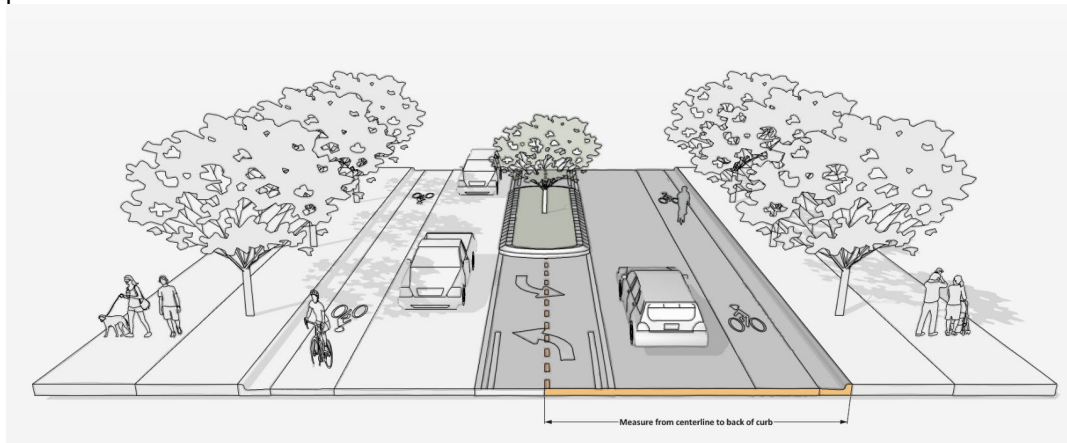
### Outstanding Issues

**Strikethrough = Resolved**

#### 1. Curblines:

- a. **Old Plank Road:** Future location of the back of curb and gutter is 24-feet from the centerline of the roadway to the back of curb.

Label and dimension the curb and gutter from the centerline for Old Plank Road on the site plan.



#### 2. Traffic Study:

A Traffic Impact Study (TIS) is not necessary for the complete review of this petition due to the site generating less than 2,500 daily trips. If during the permitting process the site generates more than 2,500 daily trips, then a traffic study will be required.

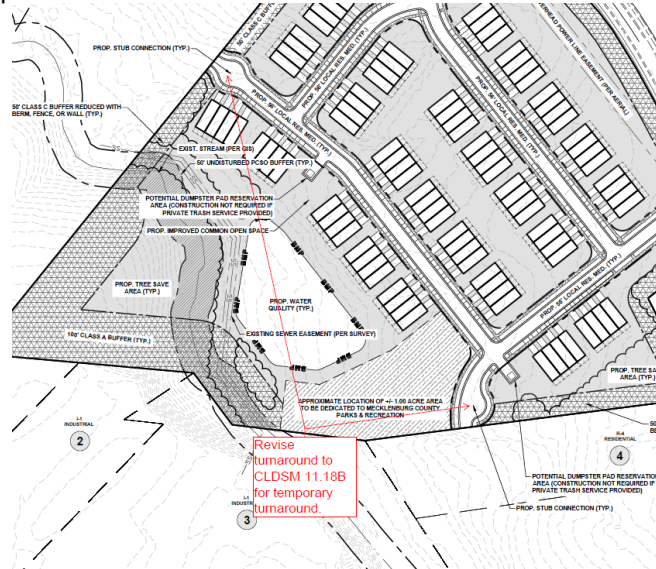
- 3. Revise site plan and conditional note(s) to commit to dedicate 35-feet of right-of-way from the road centerline of Old Plank Road per the Chapter 20 ordinance. The site plan shall label and dimension the right-of-way from the road centerline. Provide and callout sidewalk utility easement located 2-feet from the back of sidewalk along Old Plank Road if located outside of the right-of-way.
- 4. Revise site plan and conditional note(s) to commit to construct an 8-foot planting strip, and 6-foot sidewalk along all public roadways. The site plan shall label and dimension both items from the back of curb and gutter and road centerline.

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5. Revise site plan and conditional note(s) to commit to construct bicycle facilities along site frontage by locating the curb and gutter 24-feet from the center line and installing a 5-foot bike lane along Old Plank Road to meet the City Charlotte BIKE Policy.
6. Revise site plan and conditional note(s) to commit to the installation of left-turn lanes on Old Plank Road at the proposed site access points A and B. Each left turn lane should have a minimum of 100-feet of full width storage. Please be advised that additional widening may be required to install the left turn lanes.
7. Site plan and conditional note(s) revisions are needed to replace the proposed cul-de-sac bulbs to a temporary turnaround per CLSM 11.18B.



8. Revise site plan and conditional note(s) to label all roadways as public or private roadways. If the roadways will be public dimension the ROW of each road from the centerline.
9. Revise Transportation note 4 to include CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible.
10. A site plan note specifying that all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued as needed. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes.

Remove Transportation Note 6 or clarify the full scope of the proposed phasing, including which improvements are proposed under each phase. Streetscape improvements to Old Plank Road, and the proposed access roads need to be constructed prior to the site's first building certificate of occupancy. The installation of the left turn lanes on Old Plank Road could be phased if desired.

Remove or clarify the proposed phasing.

5. Prior to the issuance of the first certificate of occupancy, Petitioner shall provide the proposed internal road network to sub grade and install curb, gutter, and storm pipe/drains.

6. Unless stated otherwise herein, the Petitioner shall ensure that all transportation improvements are substantially completed prior to the issuance of the Site's fiftieth (50th) building certificate of occupancy.

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## Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.
7. The petitioner needs to complete and submit the [Right of way Abandonment Petition form](#) to CDOT for review. The decision of this rezoning petition does not correlate with the decision of the possible abandonment, as this decision is issued within the separate Right of Way Abandonment process that is controlled by North Carolina General Statutes.
8. To attain a street and pedestrian lighting recommendation, the petitioner should coordinate with Anthony Mendez, CDOT. Please visit the following link:  
<https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>