

Rezoning Transportation Analysis

Petition Number: #2022-056

General Location Identifier: 02756102, 02756103, and 02756132

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Revision Log:

Date	Description
08-24-22	First Review (PDM)

General Review Information

The petition is located adjacent east of Benfield Road, a city maintained minor thoroughfare, and south of Ridge Road, a city maintained major thoroughfare. The site is also located adjacent to Brice Knoll Lane, a city maintained local road. The petition is located within the Prosperity Church Road Corridor Wedge and within the Prosperity Hucks Area Plan. The petition is also located outside of Route 4.

Active Projects Near the Site:

- N/A

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The petition is located adjacent east of Benfield Road, a city maintained minor thoroughfare, and south of Ridge Road, a city maintained major thoroughfare. The site is also located adjacent to Brice Knoll Lane, a city maintained local road. A Traffic Impact Study (TIS) is not needed for this. The existing bike lanes, sidewalks, and planting strips on Benfield Road and Ridge Road will remain with this petition. Site plan revisions are needed to meet ordinance requirements and/or the outstanding items including, but not limited to labeling and dimension the existing roadway infrastructure, clarifying the proposed site access types, and including the CDOT standard rezoning notes. Further details are below.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Vacant	-	-	Tax Record
Entitlement with Current Zoning	Single Family Detached (R-3, 1.27 acres)	3 Dwelling Units	30	General Guidance from Planning
Proposed Zoning	Farmers Market (B-1, 1.27 acres)	1,500 SF	Not enough data to determine trip generation	Site Plan: 03/17/2022

Provide comments to the specified comments below.

Outstanding Issues

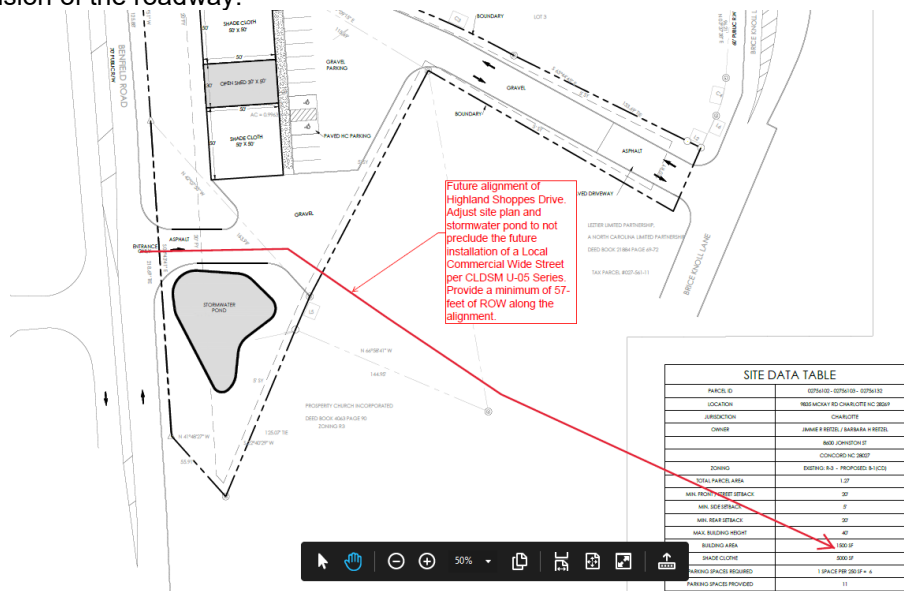
~~Strikethrough~~ = Resolved

1. Traffic Study:

A Traffic Impact Study (TIS) is not necessary for the complete review of this petition due to the site generating less than 2,500 daily trips. If during the permitting process the site generates more than 2,500 daily trips, then a traffic study will be required.

2. Revise site plan to show, callout, and dimension the existing bike lanes, curb and gutter, planting strip, sidewalk, and right-of-way on Benfield Road and Ridge Road.

3. Revise site plan and conditional note(s) to provide a minimum of 57-feet of ROW to be dedicated along the future alignment of Highland Shoppes Drive. The site plan should be revised to not preclude the future extension of the roadway.

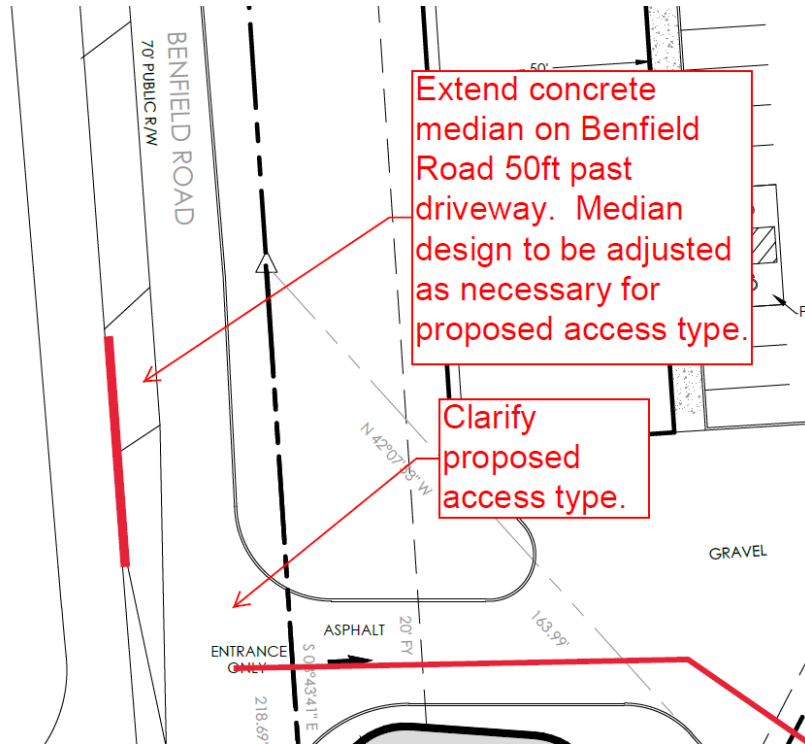


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4. Revise site plan and conditional note(s) to clarify the proposed site access type at Benfield Road. Revise site plan to extend the existing concrete median on Benfield Road 50-feet north of the proposed driveway.



5. Revise site plan and conditional note(s) to clarify the site access at Brice Knoll Lane. Clearly depict the limits of the existing road. Extend roadway within the existing/proposed ROW with a proposed access aisle with pavement section constructed to the CLDSM U-05 series for commercial wide street. Dimension the proposed ROW for Brice Knoll Lane from the centerline of the roadway.
6. A site plan note(s) specifying dedication and fee simple conveyance of all rights-of-way to the City before the site's first building certificate of occupancy is issued as needed. CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible.
7. A site plan note specifying that all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued as needed. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes.

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Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.
7. The petitioner needs to complete and submit the [Right of way Abandonment Petition form](#) to CDOT for review. The decision of this rezoning petition does not correlate with the decision of the possible abandonment, as this decision is issued within the separate Right of Way Abandonment process that is controlled by North Carolina General Statutes.
8. To attain a street and pedestrian lighting recommendation, the petitioner should coordinate with Anthony Mendez, CDOT. Please visit the following link:
<https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>