

Rezoning Transportation Analysis

Petition Number: #2022-053

General Location Identifier: 02728101

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Reviewer: Patrick Monroe
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Revision Log:

Date	Description
08-24-22	First Review (PDM)
09-22-22	Second Review (PDM)

General Review Information

The petition is located west of Prosperity Church Road, a City-maintained major thoroughfare and north of Pinewood Lane, a City-maintained local street. The petition is located within the Northeast Wedge outside of Route 4.

Active Projects Near the Site:

- Mallard Creek Road Widening (NCDOT TIP U-6028)
 - Project Scope: Widen Mallard Creek Road to 4 lanes
 - Project Limits: Mallard Creek Church Road to Breezewood Drive
 - Project Schedule: ROW 2025 / Construction 2027

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The petition is located west of Prosperity Church Road, a City-maintained major thoroughfare and north of Pinewood Lane, a City-maintained local street. A Traffic Impact Study (TIS) is not needed for this site. Site plan revisions are needed to meet ordinance requirements and/or the outstanding items including, but not limited to clarify installing an 8-foot sidewalk along the property frontage in the conditional notes and relocation of driveway to the north along Prosperity Church Road. Further details are listed below.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Single Family Detached	1 Dwelling Unit	10	<i>Tax Record</i>
Entitlement with Current Zoning	Single Family (R-3, 2.7 acres)	8 Dwelling Units	100	<i>General Guidance from Planning</i>
Proposed Zoning	Single Family Attached (R-17MF, 2.7 acres)	40 Dwelling Units	255	Site Plan: 07-21-22
Proposed Zoning	Single Family Attached (R-17MF, 2.7 acres)	51 Dwelling Units	340	<i>Site Plan: 09-12-22</i>

Provide comments to the specified comments below.

Outstanding Issues

Strikethrough = Resolved

1. ~~Curbline:~~

- a. ~~Prosperity Church Road: The future location of curb and gutter is located a minimum of 41-foot from the existing centerline to the back of curb.~~

~~Label and dimension the future location of curb and gutter from the centerline. The future location of the curb and gutter shall be shown for planning purposes only, and the existing curb and gutter along Prosperity Church Road will not be relocated with this petition.~~

2. ~~Traffic Study:~~

~~A Traffic Impact Study (TIS) is not necessary for the complete review of this petition due to the site generating less than 2,500 daily trips. If during the permitting process the site generates more than 2,500 daily trips, then a traffic study will be required.~~

3. ~~Revise site plan and conditional note(s) to commit to dedicate 50-foot of right-of-way from the road centerline. The site plan shall label and dimension the right-of-way from the road centerline.~~

4. The proposed dwelling units exceeds 12 per acre. Per Chapter 20 Subdivision ordinance, site plan and conditional note(s) revisions are needed to commit to construct an 8-foot planting strip, and 8-foot sidewalk along Prosperity Church Road. The wider sidewalk also meets the Charlotte WALKS Policy. The site plan shall label and dimension both items from the back of curb and gutter and road centerline. The 8-foot planting strip shall be located and dimensioned from the future back of curb location. Provide sidewalk utility easement or right-of-way 2-feet from the back of sidewalk for maintenance.

Clarifying Comment from 09-22-22: Revise Conditional note 1 of streetscape and landscaping to say 8-foot planting strip and 8-foot sidewalk.

Streetscape and Landscaping:

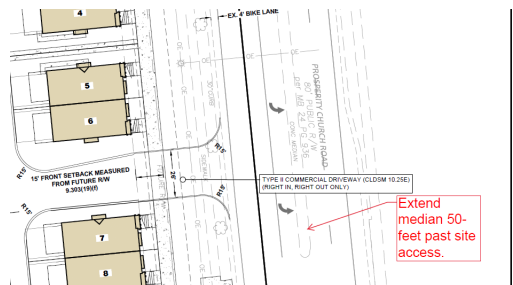
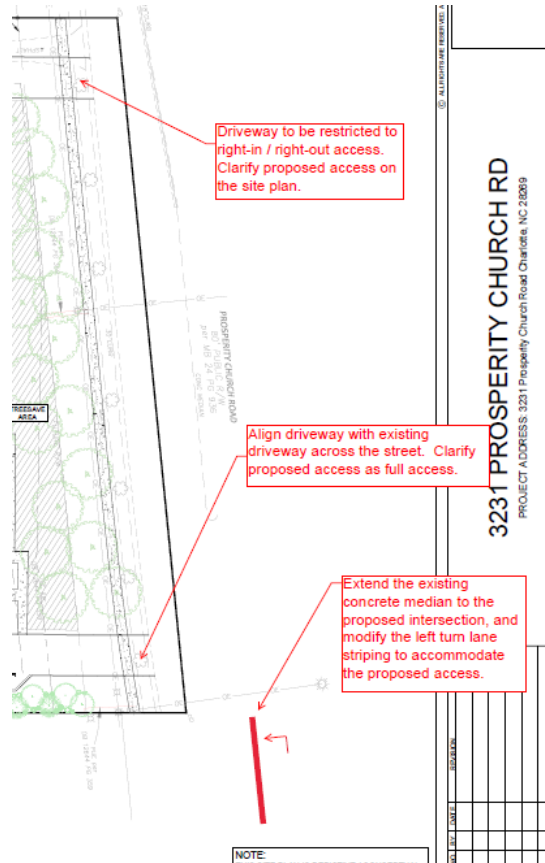
- 1) The Petitioner shall provide an 8 ft planting strip and an 5 ft sidewalk on Prosperity Church Rd as generally shown on the site plan.

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5. ~~Site plan revisions are needed to callout and dimension the existing bike lane along Prosperity Church Road.~~
6. ~~Site plan and conditional note(s) revisions are needed to identify the proposed internal alleys as privately maintained and to specify the proposed CLDSM typical section detail.~~
7. ~~Site plan and conditional note(s) revisions are needed to clarify the proposed access types. In addition, revise site plan to show existing driveways, medians, turn lanes, etc. along Prosperity Church Road. Add callout to extend the existing concrete median on Prosperity Church Road to the proposed southern access intersection and modify the left turn lane markings as necessary.~~



8. ~~Per coordination with Charlotte Fire, site plan and conditional revisions are needed to provide a fire truck turnaround at the end of the site's internal east-west roadways due to the stem length exceeding~~

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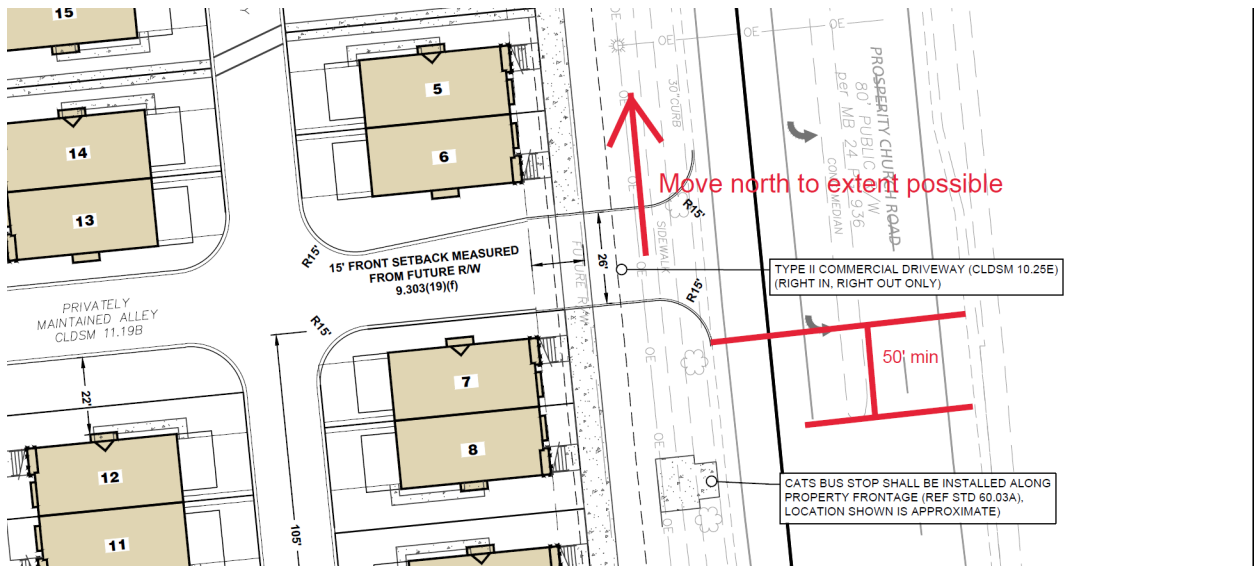
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150 feet. Alternatively, the alley stubs can be reduced to 150 feet or less to waive the turnaround requirement. Dimension the roadway stem lengths on the site plan.



9. NEW COMMENT 09-22-22: Dimension distance from radius of right-in / right-out driveway to end of median. Minimum 50' required, however CDOT requests petitioner move driveway further north to extent possible to provide adequate room for exiting site vehicles to enter left/u-turn lane.



Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align

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with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.

4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.
7. The petitioner needs to complete and submit the [Right of way Abandonment Petition form](#) to CDOT for review. The decision of this rezoning petition does not correlate with the decision of the possible abandonment, as this decision is issued within the separate Right of Way Abandonment process that is controlled by North Carolina General Statutes.
8. To attain a street and pedestrian lighting recommendation, the petitioner should coordinate with Anthony Mendez, CDOT. Please visit the following link:
<https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>