

# Rezoning Transportation Analysis

Petition Number: Insert Zoning Petition #2022-006

General Location Identifier: 02508112, 02508102, 02508101, 02507201, 02509108

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## Revision Log:

Date	Description
06-23-22	First Review (PDM)

## General Review Information

This Petition is located adjacent to Northlake Centre Parkway, a city-maintained minor thoroughfare. This petition is located within the Northlake Corridor Wedge outside of Route 4.

Active Projects Near the Site:

- RZP-2022-010 Adjacent to site location

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*CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.*

*This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.*

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Based on our review of the petition, we offer the following information for your consideration.

## Transportation Summary

This Petition is located adjacent to Northlake Centre Parkway, a city-maintained minor thoroughfare. A Traffic Impact Study (TIS) is not needed for this site, however CDOT will require a Technical Traffic Memorandum to be completed prior to the permitting approval of the full access location on Northlake Centre Parkway. Site plan revisions are needed to meet ordinance requirements and/or the outstanding items including, but not limited to committing to the installation on the proposed public streets, dimensioning and labeling the required roadway improvements, and committing to completing a TTM and turn lane improvements for a full access drive on Northlake Centre Parkway. Further details are listed below.

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## Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Vacant	-	-	<i>Tax Record</i>
Entitlement with Current Zoning	Single Family (R-3, 18.88 acres)	56 Dwelling Units	595	<i>General Guidance from Planning</i>
Proposed Zoning	Multifamily (Mid-Rise) (UR-2, 18.88 acres)	312 Dwelling Units	2,080	<i>Date: 1/14/2022</i>

**Provide comments to the specified comments below.**

### Outstanding Issues

**~~Strikethrough~~ = Resolved**

1. **Curblines:** The proposed zoning district has a setback measured from back of the existing or proposed future curblines.
  - a. **Proposed Public Streets:** Location of the back of curb and gutter will be established at 20.5-feet from the proposed centerline of the proposed public streets per CLDSM U-05A for a commercial wide street typical section.

Label and dimension the curb and gutter from the centerline for each road on the site plan.

2. **Traffic Study:**

A Traffic Impact Study (TIS) is not necessary for the complete review of this petition due to the site generating less than 2,500 daily trips. If during the permitting process the site generates more than 2,500 daily trips, then a traffic study will be required.

A Technical Traffic Memorandum (TTM) is required to be completed by the petitioner and approved by CDOT prior to the permitting approval of the proposed full access street on Northlake Centre Parkway. A conceptual drawing will also be required to be completed by the petitioner showing the feasibility of necessary turn lane improvements for full access. The petitioner will be required to permit and install all roadway improvements recommended in the CDOT approved TTM prior to the issuance of the site's first building certificate of occupancy. The access location on Northlake Centre Parkway will be restricted to right-in / right-out if the required roadway improvements cannot be installed. Revise site plan and conditional notes to commit to completing the TTM and all recommended improvements as outlined above.

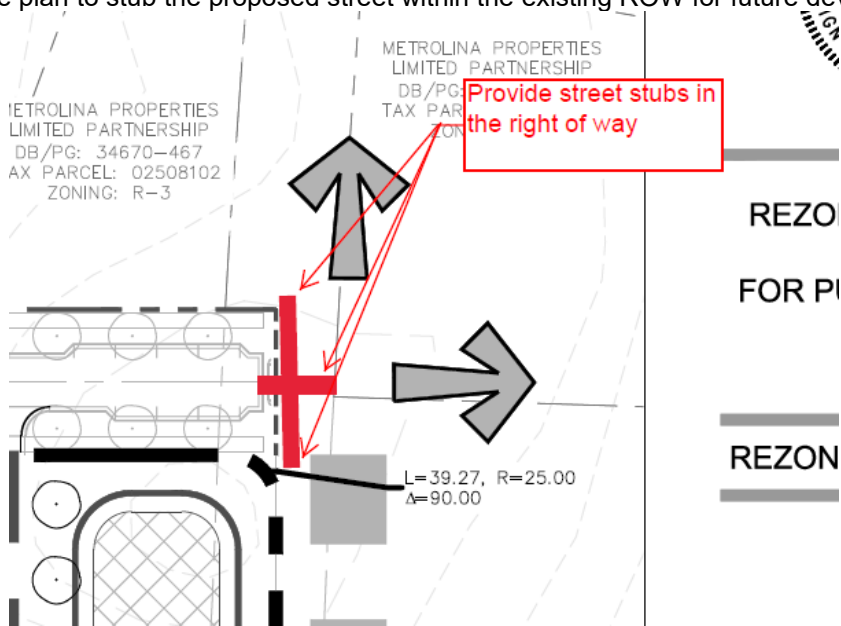
3. Revise site plan and conditional note(s) to commit to installing both public streets that are identified as "public street by others" on the site plan. A new access location on Northlake Centre Parkway will be required by CDOT for this petition. Coordinate roadway responsibilities with RZP 2022-010 as necessary.
4. Revise site plan and conditional note(s) to commit to dedicate 38.5 right-of-way from the road centerline of each public street. The site plan shall label and dimension the right-of-way from the road centerline.

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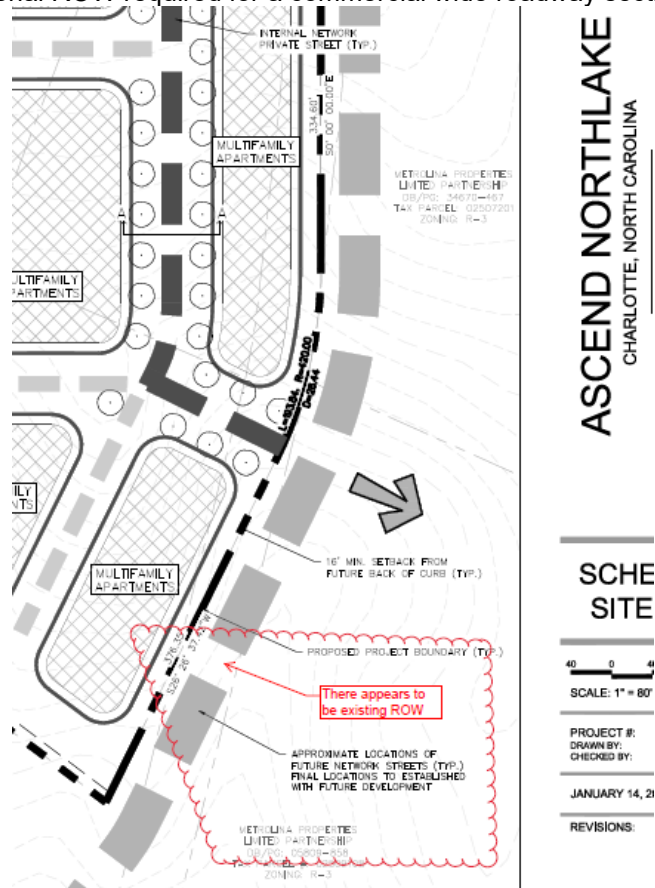
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- Revise site plan to stub the proposed street within the existing ROW for future development.



- Revise site plan and conditional notes to reconcile the existing versus proposed ROW along the east side of the property. The future street alignment should fall within existing ROW. Verify ROW width and provide any additional ROW required for a commercial wide roadway section per CLDSM U-05A.



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7. Revise site plan and conditional note(s) to commit to construct an 8-foot planting strip, and 8-foot sidewalk on the proposed public streets per Chapter 19. The site plan shall label and dimension both items from the back of curb and gutter and road centerline.
8. A site plan note(s) specifying dedication and fee simple conveyance of all rights-of-way to the City before the site's first building certificate of occupancy is issued as needed. CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible.

## Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. The setback for this district is measured from the back of the existing or future curblines as determined by CDOT and Planning during the permitting process.
2. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
3. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 50' x 50' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
4. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
5. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
6. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
7. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.
8. The petitioner needs to complete and submit the [Right of way Abandonment Petition form](#) to CDOT for review. The decision of this rezoning petition does not correlate with the decision of the possible abandonment, as this decision is issued within the separate Right of Way Abandonment process that is controlled by North Carolina General Statutes.
9. To attain a street and pedestrian lighting recommendation, the petitioner should coordinate with Anthony Mendez, CDOT. Please visit the following link:  
<https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>