

Rezoning Transportation Analysis
Petition Number: Insert Zoning Petition #2021-206
General Location Identifier: 04729144

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Revision Log:	Date	Description
	01-27-22	First Review (TM)
	02-24-22	Second Review (TM)
	03-30-22	Third Review (PDM)

General Review Information

The petition is located adjacent to Olmsted Drive, a City-maintained local street and North Tryon Street, a State-maintained major thoroughfare. The petition is located in the North East Wedge outside Route 4.

Active Projects Near the Site:

- JW Clay Blvd Streetscape
 - The project will provide sidewalks and bike lanes by widening certain areas of the existing J.W. Clay Boulevard. Sidewalks on the south and east sides of the road from W.T. Harris Boulevard to North Tryon Street.
 - Project is currently in the real estate acquisition and utility relocation phase.

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The petition is located adjacent to Olmsted Drive, a City-maintained local street and Tryon Street a State-maintained major thoroughfare. A Traffic Impact Study (TIS) is not needed for this site, because the proposed site will generate less vehicular trips than what is entitled with the current zoning/land use. The petitioner is committing to installing 8-foot planting strip and 8-foot sidewalk along the proposed private road and driveway. Site plan revisions are needed to clarify the sidewalk utility easements required along Olmsted Drive. Further details are listed below.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Vacant	-	0	Tax Record
Entitled Use	Retail	87,000 SF	5,470	General Guidance from Planning
Proposed Use	Apartments	350 Units	1,910	General Guidance from Planning
Proposed Use	Apartments Retail	305 Units 5,100 SF	2,460	Site Plan: 02/14/2022

Provide comments to the specified comments below.

Outstanding Issues

Strikethrough = Resolved

1. ~~**Curbline:** The proposed zoning district has a setback measured from back of the existing or proposed future curbline.~~
 - a. ~~**Olmsted Drive:** The future location of curb and gutter is in its existing location.~~

~~Label and dimension the curb and gutter from the centerline for each road on the site plan.~~

~~**Outstanding Comment from 01.27.2022: Dimension curb and gutter along Olmsted Drive from centerline of roadway.**~~

2. ~~**Traffic Study:**~~

~~A Traffic Impact Study (TIS) is not necessary for the complete review of this petition due to the site generating less than vehicular trips than what is entitled with the current zoning/land use.~~

3. ~~Label and dimension the existing or proposed planting strip and sidewalk along the property frontage of Olmsted Drive to provide clarity. In addition revise site plan and conditional notes to commit to installing curb ramps at the driveway connections or reconstruct the driveways as Type II Modified per GLDSM 10.25E.~~

~~**Clarifying Comment from 02.24.2022: Dimension planting strip and sidewalk along Olmsted Drive to provide clarity.**~~

4. ~~The proposed dwelling units exceeds 12 per acre. Per Chapter 20 Subdivision ordinance, site plan and conditional note(s) revisions are needed to commit to construct an 8-foot planting strip, and 8-foot sidewalk along Olmsted Drive. The wider sidewalk also meets the Charlotte WALKS Policy. In lieu of constructing the wider sidewalk at the property frontage, CDOT is requesting sidewalk is installed to connect the existing sidewalk on the frontage of the petition to the sidewalk at the corner of the Olmsted Drive and J W Clay Boulevard intersection. An 8-foot planting strip and 8-foot sidewalk is preferred.~~

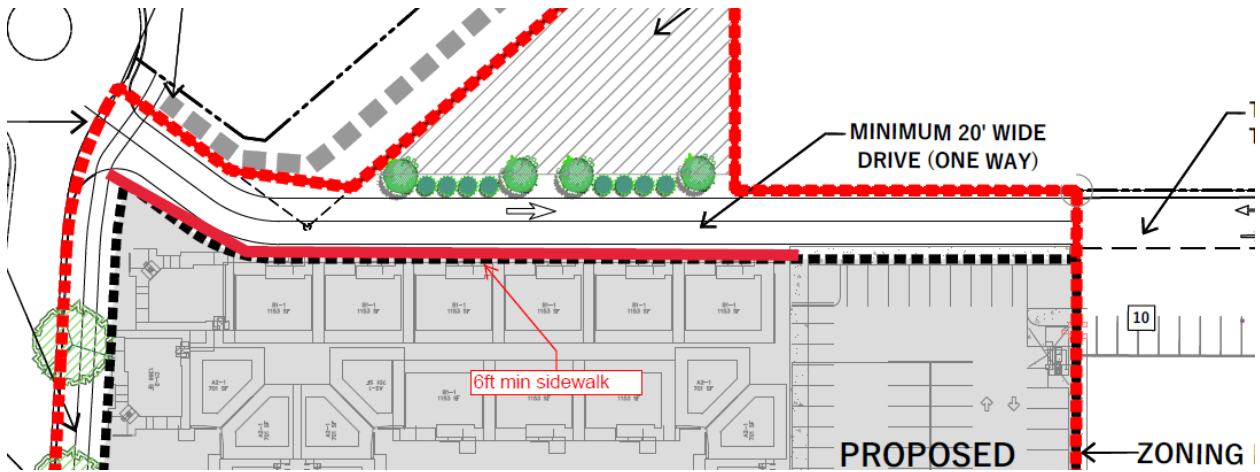
~~**Clarifying Comment from 02.24.2022: CDOT is requesting sidewalk is installed along Olmsted Drive from the frontage of petition to the intersection of J W Clay Boulevard intersection.**~~

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5. ~~Label the proposed streets as private to provide clarity to the site plan. Add conditional note stating the roads are to be private.~~
6. ~~Label and dimension the limits of the proposed public access easement on the private streets.~~
7. ~~Label and dimension the sidewalk and planting strip widths on the proposed private streets. Dimension the back of curb from the centerline of the roadways.~~

Clarifying Comment from 02.24.2022: ~~Dimension the back of curb from the centerline of the roadways.~~

8. ~~Extend the proposed sidewalk along the proposed northern street to the Olmsted Drive intersection.~~



9. **Outstanding Comment from 01.27.2022:** A site plan note specifying dedication and fee simple conveyance of all rights-of-way to the City before the site's first building certificate of occupancy is issued as needed. CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible.
Clarifying Comment from 03.30.2022: Add clarifying language to Conditional Note VIII.d to state CDOT requests sidewalk utility easement set at 2-feet behind the back of sidewalk where feasible.

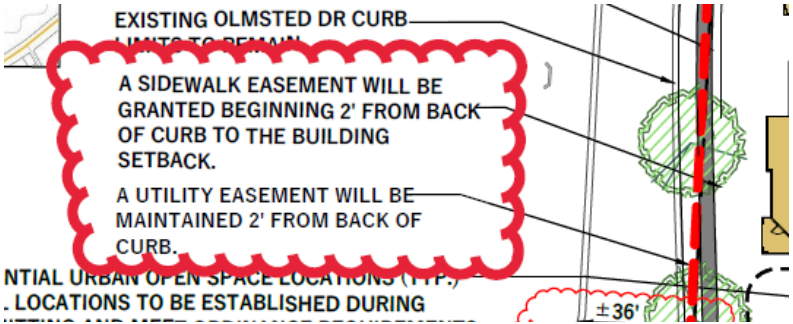
d. The existing sidewalk along Olmsted Drive shall be preserved, however a sidewalk easement shall be granted to CDOT in the areas that the sidewalk is outside of the right-of-way. CDOT requests sidewalk utility easement set a 2-feet behind back of sidewalk where feasible.

10. ~~**Outstanding Comment from 01.27.2022:** A site plan note specifying that all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued as needed. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes.~~

11. ~~**Outstanding Comment from 01.27.2022:** Add conditional note "All public roadway improvements will be subject to the standards and criteria of CDOT and NCDOT, as applicable, to the roadway improvements within their respective road system authority. It is understood that such improvements may be undertaken by the Petitioner on its own or in conjunction with other development or roadway projects taking place within the broad north eastern Mecklenburg area, by way of a private/public partnership effort or other public sector project support."~~

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- 12. **New Comment from 03.30.2022:** Revise site plan to clarify location of the sidewalk utility easement along Olmstead Drive. Sidewalk utility easement should be set a 2-feet behind the back of sidewalk in locations where the sidewalk is outside of the ROW.



Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

- 1. The setback for this district is measured from the back of the existing or future curbline as determined by CDOT and Planning during the permitting process.
- 2. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
- 3. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
- 4. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
- 5. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
- 6. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
- 7. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.
- 8. The petitioner needs to complete and submit the [Right of way Abandonment Petition form](#) to CDOT for review. The decision of this rezoning petition does not correlate with the decision of the possible abandonment, as this decision is issued within the separate Right of Way Abandonment process that is controlled by North Carolina General Statutes.
- 9. To attain a street and pedestrian lighting recommendation, the petitioner should coordinate with Anthony Mendez, CDOT. Please visit the following link: <https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>