

# Rezoning Transportation Analysis

Petition Number: 2021-160

General Location Identifier: 22348101

**From: Brandon Brezeale, PE**

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**Reviewer:**

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## Revision Log:

Date	Description
08-20-2021	First Review (WB)
09-17-2021	Second Review (WB)
10-18-2021	Third Review (WB)
11-22-2021	Fourth Review (WB)
12-23-2021	Fifth Review (AO)
01-25-2022	Sixth Review (AO)

## General Review Information

The petition is located adjacent to North Community House Road, a City-maintained major thoroughfare and Johnston Road, a City-maintained major thoroughfare. The petition is in the South Wedge and outside Route 4. Applicable area plans include the I-485 Interchange Analysis.

## Active Projects Near the Site:

- N/A

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*CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the Transportation Action Plan (TAP), Vision Zero, Urban Street Design Guidelines (USDG), Center, Corridor and Wedges, Charlotte BIKES, Traffic Impact Study Guidelines and Charlotte WALKS. With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.*

*This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.*

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Based on our review of the petition, we offer the following information for your consideration.

## Transportation Summary

The site is located on North Community House Road, a City-maintained major thoroughfare, and Johnston Road, a City-maintained major thoroughfare. The project is located on 73.97 acres and proposes two development areas with land uses as shown in the trip generation table below. Per the City's Traffic Impact Study (TIS) Guideline's thresholds, a the final TIS was submitted on July 22, 2021 and was approved by CDOT on September 7, 2021. The TIS-Recommended transportation improvements, detailed on page RZ-2 of the Rezoning site plan include signal installations, signal modifications and intersection reconfigurations along Johnston Road and Community House Road.

The site plan commits to providing internal public streets and pedestrian connectivity throughout the site in accordance with the City's Ordinances and Charlotte WALKS Policies. The petitioner also commits to constructing a 12-foot multi-use path along Johnston Road and sidewalks along both sides of the proposed local public streets per Chapter 19 and 20 of the City's Ordinances and in accordance with the Charlotte BIKES Policy.

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Site plan and conditional notes revisions are needed to specify the referenced transportation improvements and responsible party as mentioned in Section IV. Transportation. Further details are listed below.

## Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Vacant	-	-	-
Entitlement with Current Zoning	Townhomes	429 Units	3,205	RZ 2006-108
Proposed Zoning	Apartments Townhomes High School	380 Units 20 Units 2,500 students	3,885	Site Plan: 06-18-21
Proposed Zoning	Apartments High School	420 Units 2,500 students	3,995	Site Plan: 09-13-21

Provide comments to the specified comments below.

### Outstanding Issues

**Strikethrough = Resolved**

#### 1. ~~Curblines:~~

- a. ~~**North Community House Road:** The existing curb and gutter is in its future location. No curb relocation required by CDOT.~~
- ~~**Johnston Road:** The existing curb and gutter is in its future location. No curb relocation required by CDOT.~~

~~Label and dimension the curb and gutter from the centerline for each road on the site plan.~~

#### 2. ~~Traffic Study:~~

~~A Traffic Impact Study is necessary for the complete review of this petition due to the site generating more than 2,500 daily trips.~~

~~Staff will receive the petition and begin review, but the public hearing will not be scheduled until the TIS is approved to allow for the minimum time necessary for CDOT to review the study and reach agreement with the petitioner on the required transportation commitments. Any agreed upon commitments shall be included on the last site plan submitted no fewer than 4 weeks prior to the public hearing. Therefore, additional traffic mitigation comments may be forthcoming once the TIS is approved by CDOT (and NCDOT).~~

- 3. ~~Site plan and conditional note(s) revisions are needed to commit to construct a 12' multi-use path from the Barrette Place Stub into the site and northward, toward the greenway, in accordance with the Charlotte BIKES and Charlotte WALKS Plans.~~

#### 4. **Comment Clarification** (based on Site Plan dated 11-15-2021)

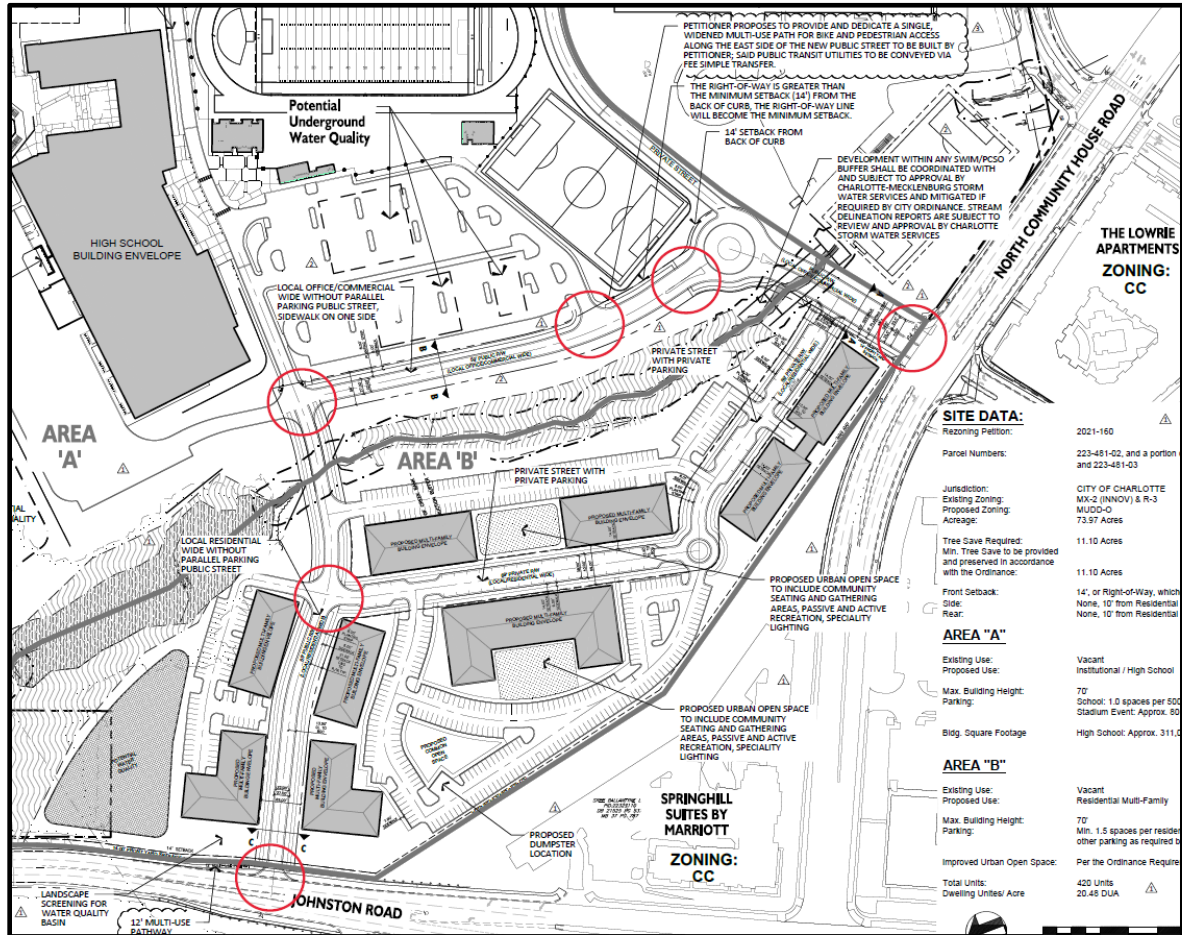
~~Revise site plan and conditional note(s) to commit to construct an 8-foot planting strip, and 6-foot sidewalk on all proposed public local roads per Chapter 19 and 20 and in accordance with the Charlotte Walks Plan.~~

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Revise site plan and conditional note(s) to commit to installing accessible curb ramps at every intersection per PROWAG and ADA law. The site plan shall label and dimension both items from the back of curb and gutter and road centerline.



- New Comment (based on Site Plan dated 10-11-2021)**  
Site plan and conditional note(s) revisions are needed to commit to construct bicycle facilities by installing a 12' multi-use path with an 8' planting strip along the site's Johnston Road frontage to meet the City of Charlotte BIKES Policy.
- Clarifying comment (based on Site Plan dated 10-11-2021):** Revise site plan and conditional note(s) to identify, depict, label, dimension, and itemize all proposed transportation-related infrastructure improvements. The conditional notes should include commitments to fully construct and fund all TIS-recommended and ordinance-required transportation improvements with applicable phasing and funding sources identified. **Specify the transportation improvements necessary to access the school as mentioned in Transportation Note #8.**
- Add conditional note that all public transportation improvements will be constructed and funded by the residential development before the residential site's first building certificate of occupancy is issued.

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8. ~~Resolved (New Site Plan 1-20-22): Remove “on site roundabout and roadway along the school frontage” from Transportation Note #7~~

△ 7. Unless stated otherwise herein, the Petitioner(s) shall ensure that all transportation improvements are substantially completed prior to the issuance of the first building certificate of occupancy for the development area for which the transportation improvement(s) relates to. Identified transportation improvements necessary to access the school shall be completed prior to the school being placed in operation. This shall include the entrance from N. Community House Rd, improvements at the intersection of Toringdon and on N. Community House Rd, proposed signal, ~~on site roundabout and roadway along the school frontage.~~ △

9. **New Comment (New Site Plan 1-20-22): Add improvement responsibility for each of the TIS recommended improvements at each intersection under Section IV.5 in the conditional notes.**

5. As recommended in the Traffic Impact Study, the following improvements shall be completed:

a. At the intersection of N Community House Road and Endhaven Lane:

Improvement responsibility: Woodfield

i. Maximize storage of the southbound left turn lane along N Community House Road; and

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b. At the intersection of N Community House Road and Toringdon Way/Site Access 1:

Improvement responsibility: CMS

i. Signalize intersection and coordinate with two signals at other study intersections along N Community House Road;

ii. Reduce taper of westbound left turn lane along N Community House Road to 50 feet;

iii. Provide a northbound left turn lane with 200' of storage and appropriate taper along Toringdon Way

iv. Convert the northbound left turn lane to a through lane with full storage along Toringdon Way;

v. Provide two ingress lanes on the site access;

vi. Maximize storage of the eastbound left turn lane along N Community House Road;

vii. Signalization will require constructing or upgrading ADA-compliant pedestrian facilities (including ramps, APS pushbuttons, and crosswalks) for crossing all approaches. Signalization will also require tie-in with the existing fiber optic cable on N Community House Road.

d. At the intersection of Johnston Road at Site Access 2: △ △

Improvement responsibility: Woodfield

i. Provide a southbound left turn lane with 100 feet of storage and appropriate taper, or as otherwise coordinated with CDOT during the permitting phase of development based upon reasonable site constraints.

10. **New Comment (New Site Plan 1-20-22): Revise conditional note under Section IV.7 by revising the final sentence to read: “...as it relates to the responsible party listed in Section IV.5”**

△ i. Provide a southbound left turn lane with 100 feet of storage and appropriate taper, or as otherwise coordinated with CDOT during the permitting phase of development based upon reasonable site constraints.

6. Petitioner(s) shall dedicate all rights-of-way where necessary, in fee simple conveyance to the City of Charlotte prior to the issuance of the first building certificate of occupancy for the development area for which the right(s)-of-way relates to.

△ 7. Unless stated otherwise herein, the Petitioner(s) shall ensure that all transportation improvements are substantially completed prior to the issuance of the first building certificate of occupancy for the development area for which the transportation improvement(s) relates to. Identified transportation improvements necessary to access the school shall be completed prior to the school being placed in operation. This shall include the entrance from N. Community House Rd, improvements at the intersection of Toringdon and on N. Community House Rd, and proposed signal. △ △

V. Architectural Standards

the responsible party listed in Section IV. 5.

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## Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.
7. The petitioner needs to complete and submit the Right of way Abandonment Petition form to CDOT for review. The decision of this rezoning petition does not correlate with the decision of the possible abandonment, as this decision is issued within the separate Right of Way Abandonment process that is controlled by North Carolina General Statutes.
8. To attain a street and pedestrian lighting recommendation, the petitioner should coordinate with Anthony Mendez, CDOT. Please visit the following link:  
<https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>