

Rezoning Transportation Analysis

Petition Number: 2021-092

General Location Identifier: 12520150, 12520154, 12520158, 12520105, 12520107, 12520110, 12520111, 12520155, 12520163

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Reviewer: Kevin Parker
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Revision Log:

Date	Description
06-01-2021	First Review (KP)

General Review Information

This site is located at the southeast corner of McDowell Street and East Morehead Street, two major thoroughfare roads, extending along the south side of McDowell street, encompassing the city-maintained Baxter Street, to the I-277 overpass. Additionally, this site is located inside Route 4 and within the limits of the Midtown/Cherry Area Plan and the Center City 2020 Vision Plan.

Active Projects Near the Site:

- Uptown Cycle Link/Belk Greenway - This segment of the connector will link pedestrians and bicyclists between the Little Sugar Creek Greenway/Cross Charlotte Trail and Irwin Creek Greenway through uptown Charlotte. The project begins at the intersection of Pearl Park Way and Kenilworth Avenue and continues along Baxter Street, in tandem with the Pearl Street Park. The project continues up McDowell Street, under I-277 and ends at the intersection of East Stonewall Street and McDowell Street.
 - <https://charlottenc.gov/Projects/Pages/BelkConnectorBaxterStonewall.aspx>
- Pearl Parkway Extension – This project will extend Pearl Parkway northwest from the existing stub at Kenilworth Avenue to existing Baxter Street. This project will construct a portion of the Belk Greenway Connector along with on-street parking and additional pedestrian infrastructure.
 - Project is currently under construction with an anticipated completion date of late 2021.
- Pedestrian Safety Program/Caldwell Street and Morehead Street Traffic Signal - A new traffic signal has been approved by the North Carolina Department of Transportation for installation at the intersection of Caldwell Street and Morehead Street. The signal is being installed to accommodate existing and future traffic, to provide improved pedestrian and bicycle crossings of Morehead St. and to create better network traffic flow.
 - This project is currently in construction with project completion in 2021.
- CityLYNX Gold Line Phase II - This phase extends the current streetcar line from the Charlotte Transportation Center west to French Street in the Historic West End and east from Novant Hospital to Sunnyside Avenue in Elizabeth.
 - Phase 2 is currently under construction and is expected to open to revenue service in 2021.

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

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Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

This 14-acre site is located on multiple blocks, extending east along McDowell Street, encompassing the City-maintained Baxter Street, to the I-277 overpass, and at the southeast corner of McDowell Street and East Morehead Street (two major thoroughfare roads). This site is located approximately ½ mile from the Blue Line’s Stonewall and Carson Transit Stations, and approximately ½ mile from the CityLYNX Gold Line’s McDowell Street Transit Station. This site is located approximately ½ mile from the Little Sugar Creek Greenway’s entrance in Midtown at Pearl Parkway.

The petitioner submitted a Scope for a Transportation Impact Study (TIS), as the proposed project exceeds 2,500 vehicle trips/day. The TIS Scope was approved on May 13, 2021. The TIS shall include the implementation of specific transportation improvements, including Transportation Demand Management (TDM) strategies. These TDM strategies will encourage commuters to use numerous methods of multi-modal transportation that seek to mitigate the number of single-occupancy vehicles commuting to/from the site. One of the TDM strategies the petitioner has already committed to is constructing a cycle track that will connect to the Pearl Parkway Extension and extend along McDowell Street to Stonewall Street, and tie-into the Brooklyn Village Project. CDOT is coordinating with the petitioner to incorporate TDM strategies that will coincide with the City’s BIKES, WALKS, and Vision Zero’s policies to ensure safe and efficient multi-modal transportation. Also, CDOT has been coordinating with the petitioner to implement a “Scramble Crosswalk”, cross section shown on Rezoning Sheet RZ-1.1, to utilize unique pedestrian movements that promotes safe and efficient pedestrian activity. The petitioner shall submit a full TIS for review and processing by CDOT, NCDOT and CATS before this petition proceeds to public hearing.

Site plan revisions are needed to meet ordinance requirements and the outstanding items including, but not limited to, TIS completion and approval, verifying the dimensions for the cross-sections associated with the proposed street network, and including additional clarification language associated with the bicycle and pedestrian network. Further details are listed below.

Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	General Office Medical Office	211,345 SF 21,604 SF	2102	Tax Record
Entitlement with Current Zoning	General Office General Office General Office (O-2 1.9 acres) Retail (B-2 4.6 acres) Retail (B-1 0.85 acres)	107,134 SF 83,200 SF 28,500 SF 69,000 SF 8,500 SF	8,135	RZ 2000-61, RZ 2005-090, and General Guidance from Planning
Proposed Zoning	General Office College (LUC 550) Mid-Rise (LUC 231) Hotel	393,000 SF Medical School 1,000 Units 600 Rooms	15,590	Site Plan: 03-26-21

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Provide comments to the specified comments below.

Outstanding Issues

~~Strikethrough~~ = Resolved

1. **Curb line:** The proposed zoning district has a setback measured from back of the existing or proposed future curb line. Revise Sheet RZ-1.1 by adding the curb line dimensions to the cross sections provided. Please note: The following curb line dimensions proposed are conceptual and may be altered as coordination with CDOT throughout the rezoning process ensues.
 - a. **Morehead Street:** The location of curb and gutter will be moved and located 38-feet from the existing centerline to the back-of-curb.
 - b. **McDowell Street:** The future location of curb and gutter is in the existing location.
 - c. **Baxter Street (Section C-C)** – The location of curb and gutter will be moved and located to 21.5-feet from the existing centerline to each back-of-curb.
 - d. **Baxter Street (Section D-D)** – The location of curb and gutter will be moved and located to 19-feet from the existing centerline to each back-of-curb.
 - e. **Baxter Street Extension (Section E-E)** – The location of curb and gutter will be moved and located to 21.5-feet from the existing centerline to each back-of-curb.
 - f. **Proposed Public Street (Section F-F)** - The location of curb and gutter will be located 18.5-feet from the proposed centerline to each back-of-curb.

2. **Traffic Study:**

A Traffic Impact Study is necessary for the complete review of this petition due to the site generating more than 2,500 daily trips.

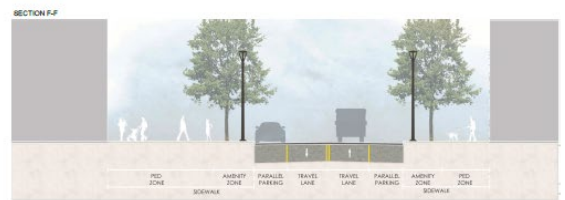
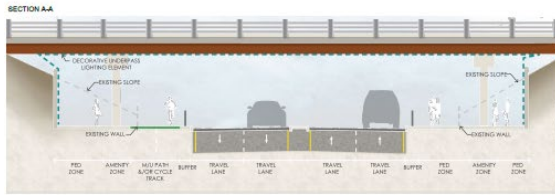
Staff will receive the petition and begin review, but the public hearing will not be scheduled until the TIS is approved to allow for the minimum time necessary for CDOT to review the study and reach agreement with the petitioner on the required transportation commitments. Any agreed upon commitments shall be included on the last site plan submitted no fewer than 4 weeks prior to the public hearing. Therefore, additional traffic mitigation comments may be forthcoming once the TIS is approved by CDOT (and NCDOT).

3. **Right-of-way and Other Infrastructure Dimensions:** Revise Rezoning Sheet RZ-1.1 to include detailed dimensions for each infrastructure item, and the total right-of-way for the cross sections shown. Refer to the USDG Cross sections and on-street parking details, located within the Charlotte Land Development Standards Manual, to determine the appropriate widths associated with each infrastructure item and the cross sections' total widths. Revise the site plan sheets by clearly labeling each public and private street section as well as each access point. Labeling each street section and access point will allow for thorough comments throughout the rezoning and permitting process. CDOT prefers the separated cycle track and sidewalk for McDowell Street (cross-sections A-A and B-B). Please note: The following right-of-way dimensions proposed are conceptual and may be altered as coordination with CDOT throughout the rezoning process ensues.
 - a. **Morehead Street:** 56-feet of right-of-way dedication measured from the existing road centerline.
 - b. **McDowell Street:** 110-feet of total right-of-way dedication, 55-feet measured from the existing road centerline to each right-of-way line.
 - c. **Baxter Street (Section C-C)** – 93-feet of total right-of-way dedication, 46.5-feet measured from the existing road centerline to each right-of-way line.
 - d. **Baxter Street (Section D-D)** – 86-feet of total right-of-way dedication, 43-feet measured from the existing road centerline to each right-of-way line.
 - e. **Baxter Street Extension (Section E-E)** – 93-feet of total right-of-way dedication, 46.5-feet measured from the existing road centerline to each right-of-way line.
 - f. **Proposed Public Street (Section F-F)** - 73-feet of total right-of-way dedication, 36.5-feet measured from the existing road centerline to each right-of-way line.

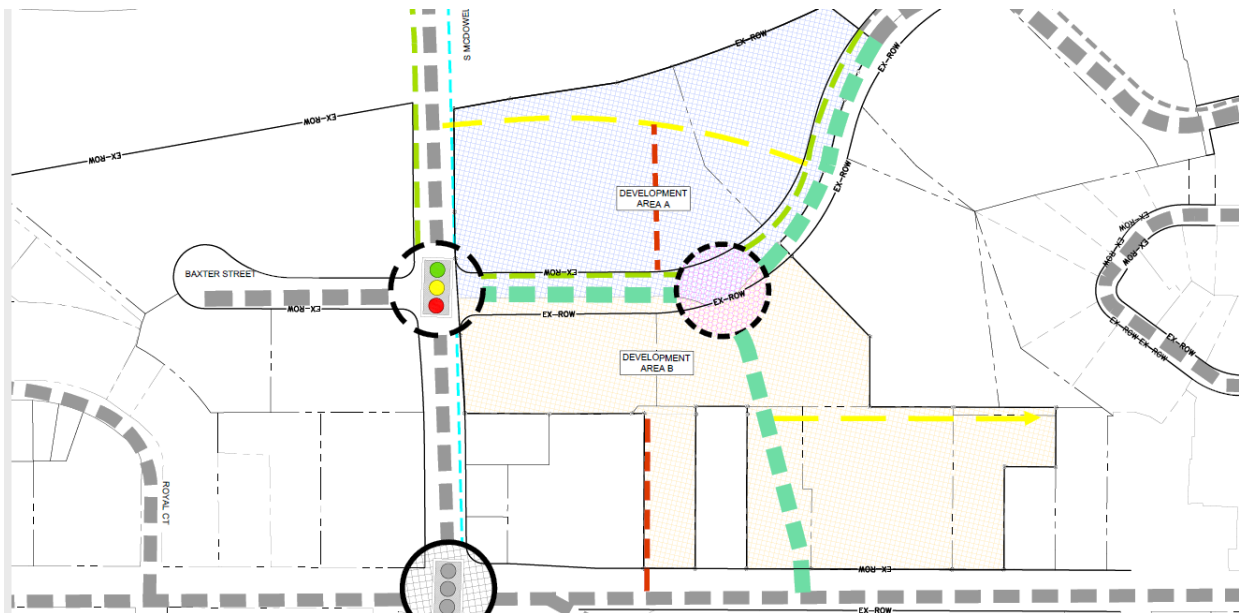
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Sheet RZ-1.1 – Typical Sections



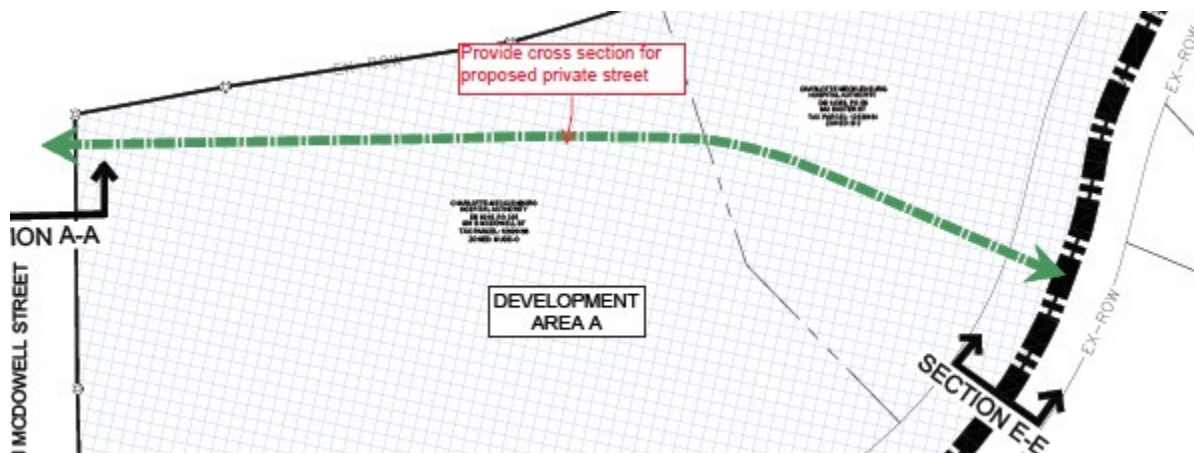
Sheet RZ-1.3 – Ped and Bike Connectivity

4. Revise the Rezoning Sheet RZ-1.0 and RZ-1.1 by establishing a cross section associated with the proposed private street connecting McDowell Street to Baxter Street Extension. Include the cross section on Rezoning Sheet RZ-1.1.

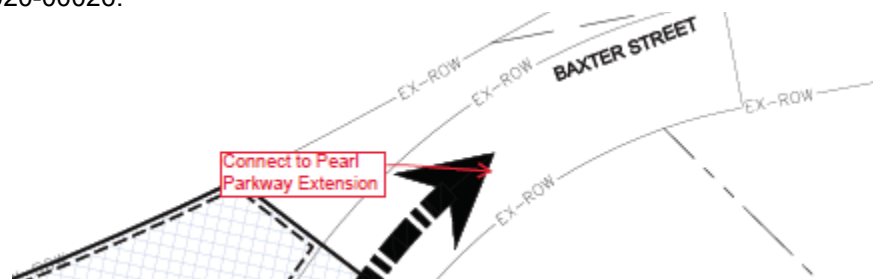
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5. Revise the site plan sheets and conditional notes by committing to connect the roadway improvements, pedestrian improvements, and the proposed cycle track associated with the Baxter Street Extension, Cross Section E-E, to the relative infrastructure associated with the Pearl Parkway Extension, SDRMUR-2020-00026.



6. Revise the conditional notes by including a note stating: Any improvements within NCDOT right-of-way, i.e. I-277 Underpass Improvements and Morehead Street Signalized Pedestrian Crossing, will be subject to NCDOT approval during the permitting process.
7. Revise the site plan and conditional notes to include Advisory Information, Notes 7 and 9, in regards to Encroachment Agreement requirements for non-standard item(s) proposed within the public right-of-way and the street and pedestrian lighting process.
8. Site plan note(s) specifying dedication and fee simple conveyance of all rights-of-way to the City and NCDOT before the site's first building certificate of occupancy is issued is needed. CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible.
9. A site plan note specifying that all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued is needed. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes.

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

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1. The setback for this district is measured from the back of the existing or future curbline as determined by CDOT and Planning during the permitting process.
2. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
3. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
4. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
5. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
6. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
7. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.
8. The petitioner needs to complete and submit the [Right of way Abandonment Petition form](#) to CDOT for review. The decision of this rezoning petition does not correlate with the decision of the possible abandonment, as this decision is issued within the separate Right of Way Abandonment process that is controlled by North Carolina General Statutes.
9. To attain a street and pedestrian lighting recommendation, the petitioner should coordinate with Anthony Mendez, CDOT. Please visit the following link:
<https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>