

Rezoning Transportation Analysis

Petition Number: 2021-089

General Location Identifier: 10503127

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Reviewer:

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Revision Log:

Date	Description
08-20-2021	First Review (WB)
10-15-2021	Second Review (AO)

General Review Information

The petition is for a site located on Rocky River Road, a State-maintained major thoroughfare and John Russell Road, a State-maintained minor thoroughfare. The petition is in the East Wedge outside Route 4. Applicable area plans include the Newell Small Area Plan.

Active Projects Near the Site:

- Grier Road/Rocky River Road Left-Turn Lane
 - Project will add a left turn lane from eastbound Grier Road to northbound Rocky River Road.
 - Phase: Construction (expected completion late 2021)
 - GS PM: Will Scott
 - Website: <https://charlottenc.gov/Projects/Pages/GrierRockyRiver.aspx>

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the Transportation Action Plan (TAP), Vision Zero, Urban Street Design Guidelines (USDG), Center, Corridor and Wedges, Charlotte BIKES, Traffic Impact Study Guidelines and Charlotte WALKS. With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located on Rocky River Road, a state-maintained major thoroughfare, and John Russel Road, a state-maintained minor thoroughfare. A Traffic Impact Study (TIS) is not necessary for the complete review of this petition due to the site generating less than 2,500 daily trips. If the site generates more than 2,500 daily trips, during the permitting process, then a traffic study will be required. Petitioner has committed to dedicating right-of-way along the site's frontage, providing a 100-foot right turn lane at the site's access on Rocky River Road per the NCDOT's standards, installing the curb and gutter 35 feet from the Rocky River Road centerline, constructing a 12-foot multi-use path along John Russel Road, striping the fourth crosswalk leg and upgrading the pedestrian signal equipment at the John Russel Road and Rocky River Road intersection, and providing a 6-foot sidewalk with an 8-foot planting strip along Rocky Road while filling in the sidewalk gap between the site and the existing sidewalk to the West. In lieu of a road extension from the Lupine Court Stub, and to avoid impacts to wetlands, the petitioner commits to constructing a multiuse path between Lupine Court to Yaupon Road, in accordance with City Charlotte BIKES and

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Charlotte WALKS Policies. This path will provide pedestrian and bicycle access from the adjacent neighborhoods to the Reedy Creek Park. Site plans revisions are needed to show the crosswalk width to be 10' wide unless otherwise specified. Further details are listed below:

Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Single Family	1 Dwelling	10	Tax Record
Entitled Use	Single Family (R-3 21.3 acres)	63 Dwellings	680	General guidance from planning
Proposed Use	Townhomes	76 Units	535	Site Plan: 03/22/2021

Provide comments to the specified comments below.

Outstanding Issues

Strikethrough = Resolved

1. ~~Curbline:~~

a. ~~**Rocky River Road:** The future curb and gutter shall be located 35' from centerline to back of curb. This should generally match what has been set up for future widening, west of the project's limits.~~

b. ~~**John Russell Road:** The future curb and gutter shall be located 19' from centerline to back of curb.~~

~~Label and dimension the curb and gutter from the centerline for each road on the site plan.~~

2. ~~**Traffic Study:** A Traffic Impact Study (TIS) is not necessary for the complete review of this petition due to the site generating less than 2,500 daily trips. If during the permitting process the site generates more than 2,500 daily trips, then a traffic study will be required.~~

3. ~~Revise site plan and conditional note(s) to commit to dedicate 50' (minimum) right of way from the Rocky River Road centerline. The site plan shall label and dimension the right of way from the road centerline.~~

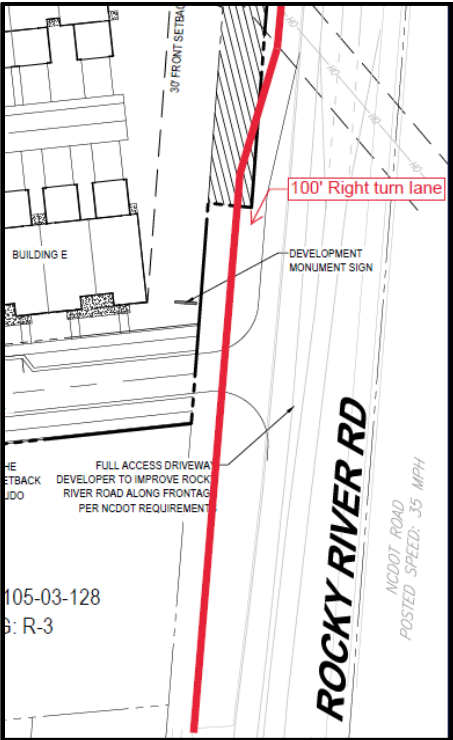
4. ~~Revise site plan and conditional note(s) to commit to dedicate 35' (minimum) right of way from the John Russel Road centerline. The site plan shall label and dimension the right of way from the road centerline.~~

5. ~~Site plan and conditional note(s) revisions are needed to commit to providing a 100' right turn lane at the site's access on Rocky River Road per the NCDOT's standards. The proposed right turn lane should be dimensioned.~~

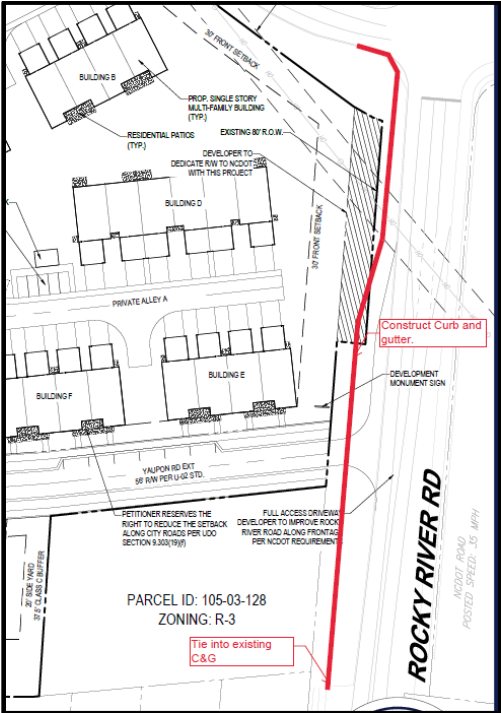
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6. ~~Site plan and conditional note(s) revisions are needed to commit to construct bicycle facilities by installing the curb and gutter 35' from the Rocky River Road centerline per the Charlotte's BIKE Policy. The proposed curblines should match what has been set up west of the project.~~

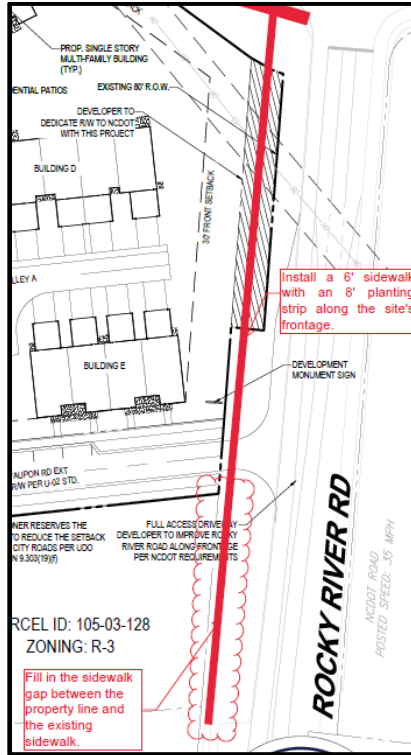


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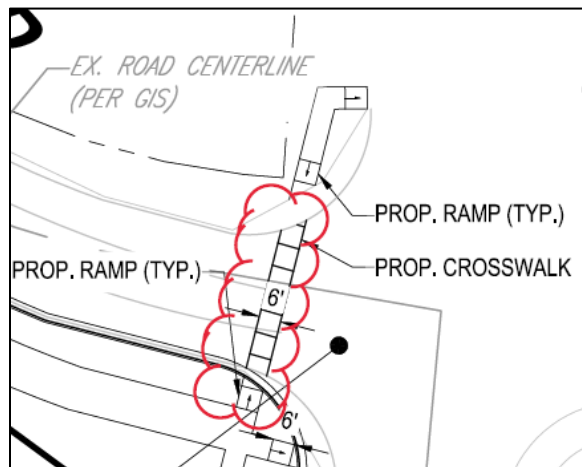
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7. ~~Revise site plan and conditional note(s) to commit to construct an 8-foot planting strip, and 6-foot sidewalk on Rocky River Road per Chapter 20. Tie into the existing sidewalk west of the site to provide pedestrian connectivity and accessibility in accordance with PROWAG and the Charlotte Walks plan. The site plan shall label and dimension both items from the back of curb and gutter.~~



8. Revise site plan and conditional note(s) to commit to construct accessible ramps and a crosswalk at the John Russel Road Intersection to provide accessibility in accordance with PROWAG and ADA. The site plan shall label and dimension all items.



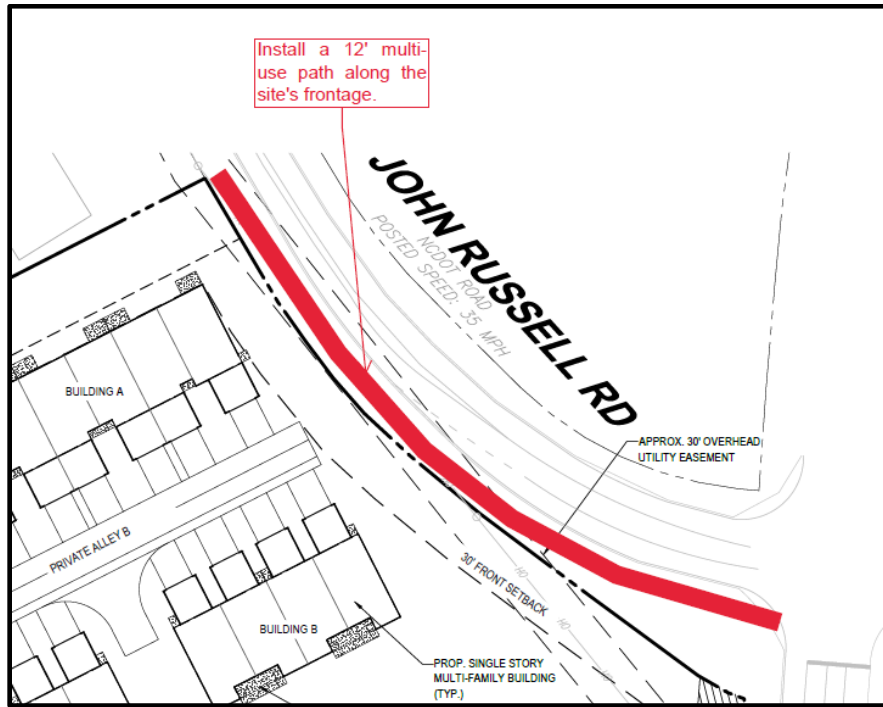
Technical Clarification (10-18-21): The crosswalk width to be 10' wide unless otherwise specified.

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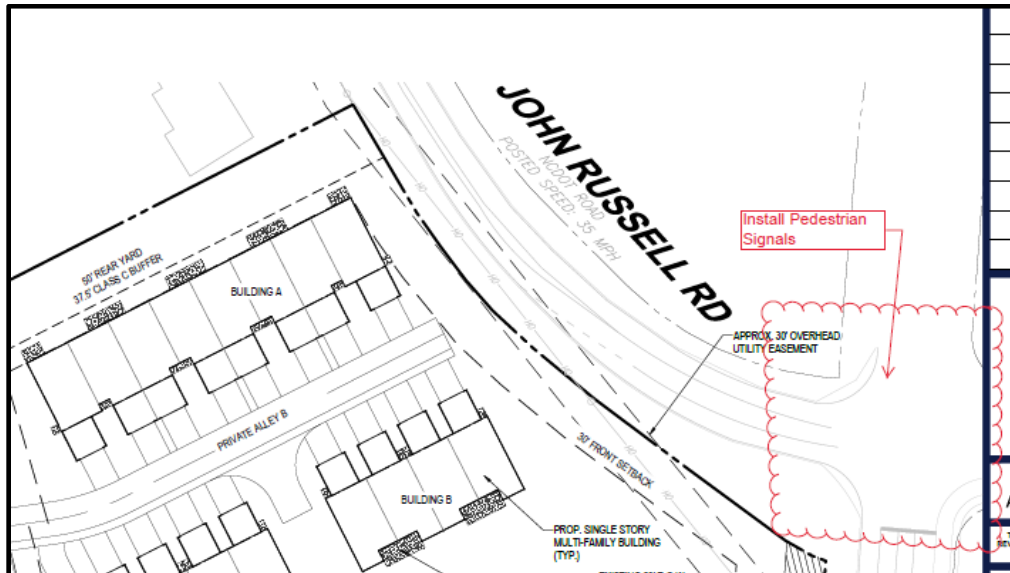
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9. ~~Site plan and conditional note(s) revisions are needed to commit to construct pedestrian and bicycle facilities by installing a 12' multi-use path along the John Russel Road Frontage in accordance with the Charlotte BIKES Policy.~~



10. ~~Site plan and conditional note(s) revisions are needed to commit to upgrade the pedestrian signal equipment at the John Russel Road and Rocky River Road intersection to meet PROWAG and ADA standards. The pedestrian signal upgrade will include APS installation and the relocation of pedestrian signals off existing wood pole~~

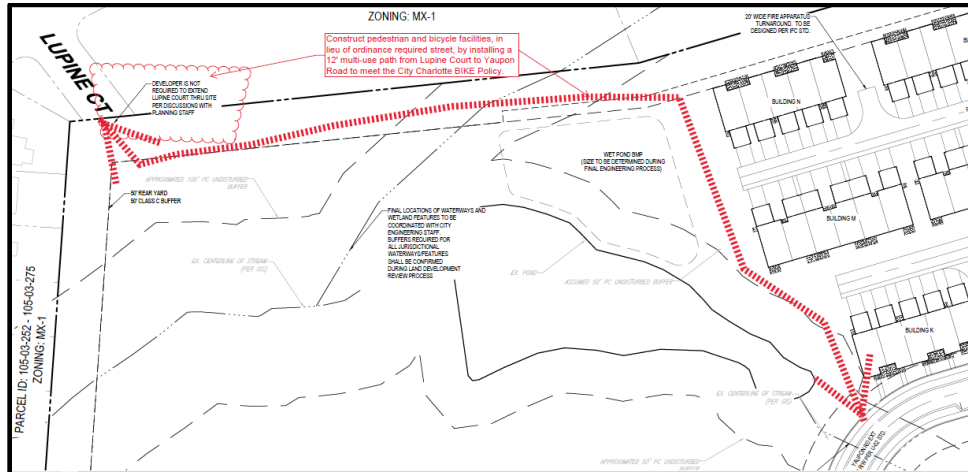


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11. Add conditional note "All public roadway improvements will be subject to the standards and criteria of CDOT and NCDOT, as applicable, to the roadway improvements within their respective road system authority. It is understood that such improvements may be undertaken by the Petitioner on its own or in conjunction with other development or roadway projects taking place within the broad north-eastern Mecklenburg area, by way of a private/public partnership effort or other public sector project support."
12. Site plan and conditional note(s) revisions are needed to commit to construct pedestrian and bicycle facilities, in lieu of ordinance required street, by installing a 12' multi-use path from Lupine Court to Yaupon Road to meet the Charlotte BIKES Policy. This path will provide pedestrian and bicycle access from the adjacent neighborhoods to the Reedy Creek Park.



Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation.

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Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.

7. The petitioner needs to complete and submit the Right of way Abandonment Petition form to CDOT for review. The decision of this rezoning petition does not correlate with the decision of the possible abandonment, as this decision is issued within the separate Right of Way Abandonment process that is controlled by North Carolina General Statutes.
8. To attain a street and pedestrian lighting recommendation, the petitioner should coordinate with Anthony Mendez, CDOT. Please visit the following link:
<https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>