

Rezoning Transportation Analysis
Petition Number: Insert Zoning Petition #2021-063
General Location Identifier: 14124101

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Revision Log:	Date	Description
	05-27-2021	First Review (KP)

General Review Information

The petition is located adjacent to Beam Road, a City-maintained minor thoroughfare, and Shopton Road, a State-maintained minor thoroughfare. The petition is in the Shopton Road Center outside Route 4. Applicable area plans include the Westside Strategy Plan Study.

Active Projects Near the Site:

- Shopton Road Extension CTP Alignment
 - An extension of existing Shopton Road providing an additional east-west minor thoroughfare connection from the existing intersection of Shopton Road and Pinecrest Drive to Beam Road. Bicycle and Pedestrian infrastructure will be included in this Shopton Road Extension.
 - There is no timeline associated with this CTP Alignment, however, this petition has committed to constructing a portion of the eastern segment, beginning at Beam Road and extending west approximately ¼ of a mile.

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located on a Beam Road and Shopton Road, two City-maintained and State-maintained minor thoroughfares respectively. The Shopton Road Extension CTP Alignment is positioned along the northern boundary of this petition, and the petitioner has committed to construct approximately ¼ of a mile of the Shopton Road Extension. Additionally, in accordance with City Ordinances and the City's BIKES, WALKS, and Vision Zero Policies, the petitioner has committed to construct the bicycle and pedestrian infrastructure along the proposed segment of the Shopton Road Extension.

Site plan revisions are needed to meet ordinance requirements and the outstanding items including, but not limited to committing to turn lane improvements at the proposed site accesses on Shopton Road and Beam Road, including additional bicycle and pedestrian infrastructure, and adding clarifying language in the conditional notes regarding right-of-way dedication. Further details are listed below.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Vacant	-	0	Tax Record
Entitled Use	Warehouse (B-D 105 acres)	1,050,000 SF	1,705	General Guidance from Planning
Proposed Use	Warehouse	1,200,000 SF	1,945	Site Plan: 3/2/2021

Provide comments to the specified comments below.

Outstanding Issues

Strikethrough = Resolved

1. Curblines:

- a. **Shopton Road:** The location of curb and gutter will be placed 27-feet from the existing road centerline to back-of-curb.
- b. **Beam Road:** The location of curb and gutter will be placed 38-feet from the existing road centerline to back-of-curb.

Label and dimension the curb and gutter from the centerline for each road on the site plan.

2. Traffic Study:

A Traffic Impact Study (TIS) is not necessary for the complete review of this petition due to the site generating less than 2,500 daily trips. If during the permitting process the site generates more than 2,500 daily trips, then a traffic study will be required.

3. Right-of-way:

- a. **Shopton Road:** Revise site plan and conditional note(s) to commit to dedicate 43-feet of right-of-way from the road centerline.
- b. **Beam Road:** Revise site plan and conditional note(s) to commit to dedicate 56-feet of right-of-way from the road centerline.

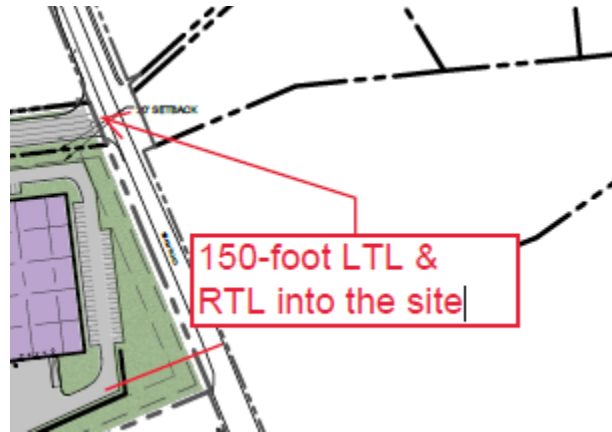
The site plan shall label and dimension the right-of-way from the road centerline.

- 4. Revise site plan and conditional note(s) to commit to construct an 8-foot planting strip, and 6-foot sidewalk along the site's frontage of Shopton Road and Beam Road, per City Ordinance - Chapter 19. The site plan shall label and dimension both items from the existing road centerline.
- 5. Revise the site plan and conditional notes to commit to constructing 8-foot buffered bike lanes (3-foot buffer and 5-foot lane) along the site's frontage of Shopton Road and Beam Road. The site plan shall label and dimension the bike lane from the existing centerline of each road.
- 6. Revise the site plan and conditional notes to commit to constructing a left-turn lane **and** right-turn lane, with 150-feet of storage and appropriate bay-taper length, at the proposed site entrance located on Beam Road. Additionally, include in the conditional note that the turn lanes will be designed and striped in accordance with the City of Charlotte's Pavement Marking Guidelines.

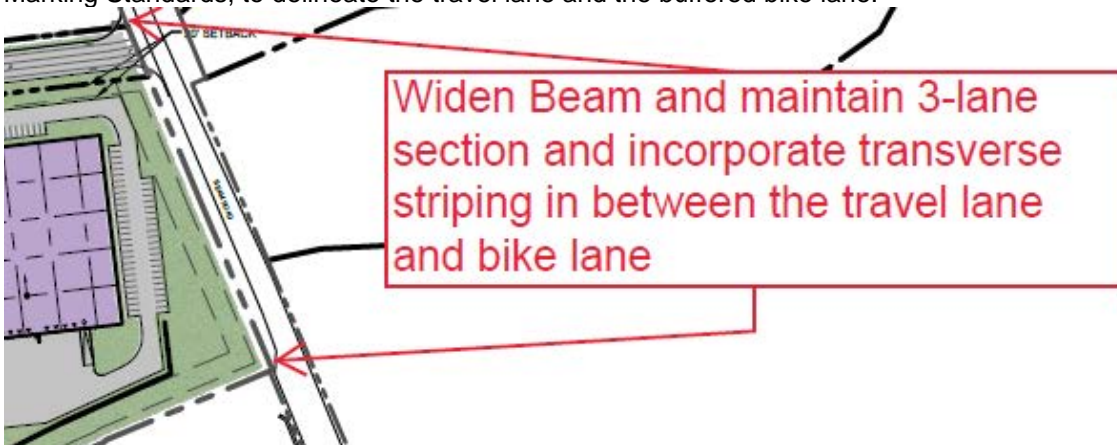
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7. Revise the site plan and conditional notes to commit to widen Beam Road 38-feet from the existing centerline to back-of-curb, along the site's frontage. Additionally, revise the striping along Beam Road to incorporate a three-lane section with the buffered bike lanes striped along the edge-of-pavement. Any additional asphalt shall utilize transverse striping, in accordance with the City's Pavement Marking Standards, to delineate the travel lane and the buffered bike lane.



8. Revise the site plan and conditional notes to commit to constructing a left-turn lane **and** right-turn lane, with 150-feet of storage and appropriate bay-taper length, at the proposed site entrance located on Shopton Road. Additionally, include in the conditional note that the turn lanes will be designed and striped in accordance with NCDOT's Roadway Design Manual.



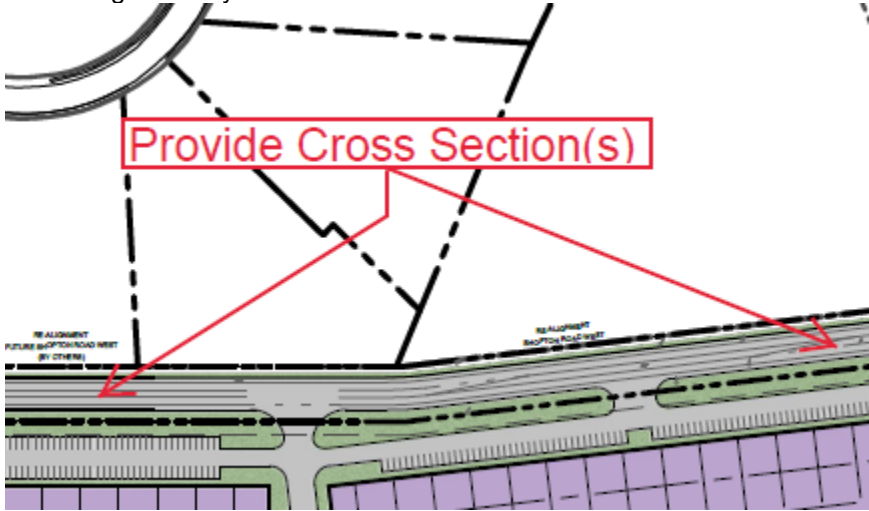
9. Revise the site plan and conditional notes by committing to coordinate driveway location and site access associated with PID# 14106101. CDOT recommends moving the driveway location to the

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proposed segment of the Shopton Road Extension. The driveway location will be subject to CDOT approval during the permitting process.

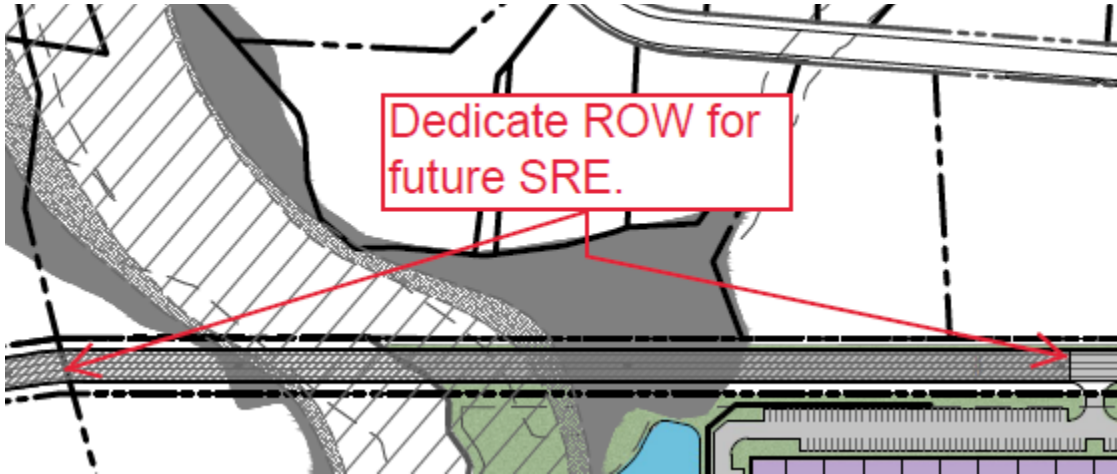


- 10. Revise the site plan and conditional notes to commit to construct a portion of the Shopton Road Extension CTP Alignment, as generally depicted on the site plan. Additionally, provide the cross section, associated with the Shopton Road Extension, that includes dimensions of the proposed infrastructure and the right-of-way to be dedicated to CDOT.

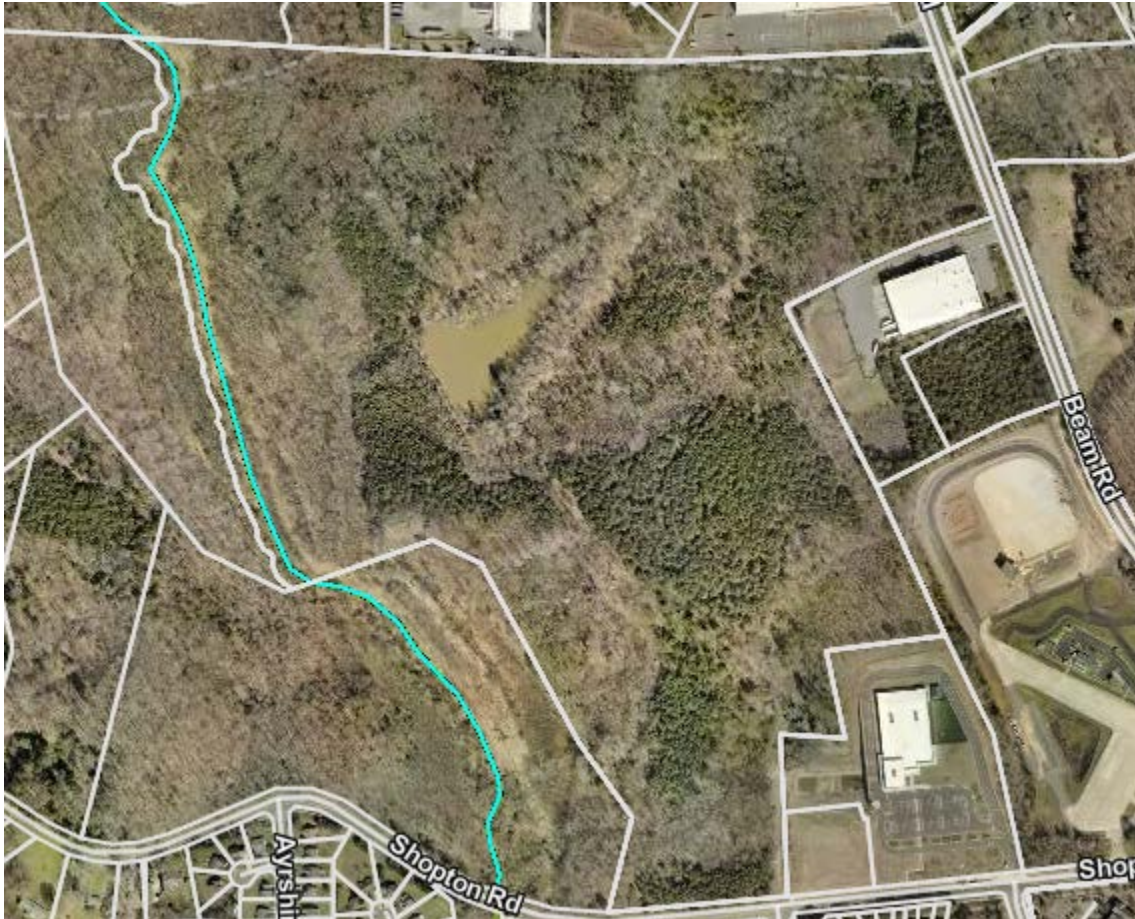


- 11. Revise the site plan and conditional notes by committing to dedicate the right-of-way for the future segment the Shopton Road Extension, that will not be constructed with this development, to CDOT. The amount of right-of-way dedication should be proportionate with the cross section established in Note 10 of this memo.

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12. Revise the site plan and conditional notes to commit to coordinate with Mecklenburg County Parks and Recreation and construct the portion of the Coffee Creek Greenway that extends through this site.



13. A site plan note(s) specifying dedication and fee simple conveyance of all rights-of-way to the City before the site's first building certificate of occupancy is issued is needed. CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible.

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14. A site plan note specifying that all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued is needed. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes.

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

1. The setback for this district is measured from the back of the existing or future curblines as determined by CDOT and Planning during the permitting process.
2. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
3. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.
4. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
5. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
6. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
7. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.
8. The petitioner needs to complete and submit the [Right of way Abandonment Petition form](#) to CDOT for review. The decision of this rezoning petition does not correlate with the decision of the possible abandonment, as this decision is issued within the separate Right of Way Abandonment process that is controlled by North Carolina General Statutes.
9. To attain a street and pedestrian lighting recommendation, the petitioner should coordinate with Anthony Mendez, CDOT. Please visit the following link:
<https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>