

Rezoning Transportation Analysis
Petition Number: Insert Zoning Petition #2021-056
General Location Identifier: 04105416

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Revision Log:

Date	Description
05-27-2021	First Review (EL)

General Review Information

The petition is located adjacent to B Avenue, a City-maintained local street, east of Beatties Ford Road, a City-maintained major thoroughfare. The petition is in the North Corridor outside Route 4. Applicable area plans include the Westside Strategy Plan.

Active Projects Near the Site:

- N/A

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located on a City-maintained local road (B Avenue), east of Beatties Ford Road, a City-maintained major thoroughfare. There is unmaintained right-of-way adjacent to the site that will need to be constructed. Site plan revisions are needed to meet ordinance requirements and the outstanding items including, but not limited to dedicating public right of way and constructing improvements for new public streets, implementing sidewalk and accessible ramps, and adding standard CDOT conditional notes. Further details are listed below.

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Trip Generation

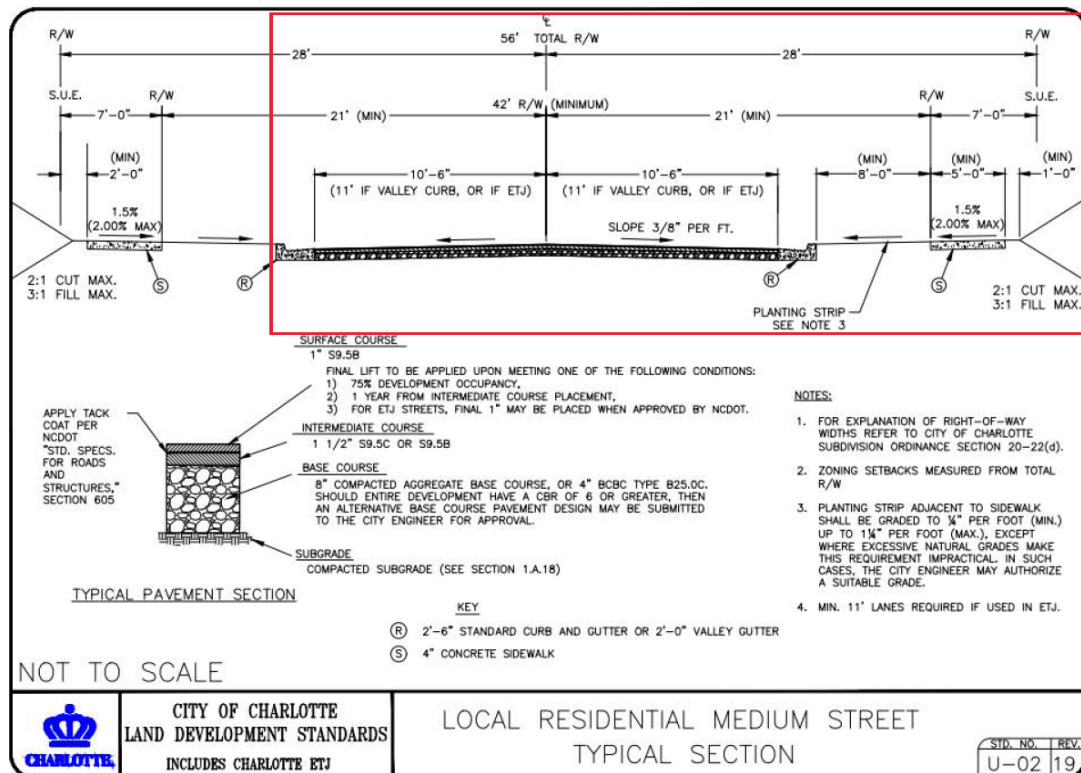
Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Single Family	2 Dwellings	20	Tax Record
Entitled Use	Single Family Apartments	2 Dwellings 12 Units	85	General Guidance from Planning
Proposed Use	Apartments	36 Units	195	Site Plan: 2/22/2021

Provide comments to the specified comments below.

Outstanding Issues

Strikethrough = Resolved

1. **Curblines:** The proposed zoning district has a setback measured from back of the existing or proposed future curblines.
 - a. **B Avenue:** Location of 2'-6" curb and gutter needs to be installed 13-feet from B Avenue road centerline per CLDSM standard detail number *U-02 – Local Residential Medium Street Typical Section*.
 - b. **North Avenue:** Construct a CLDSM standard detail number *U-02 – Local Residential Medium Street Typical Section* (for the portion highlighted in red) along the property frontage of North Avenue. Planting strip and sidewalk is only required along this rezoning site's property frontage.

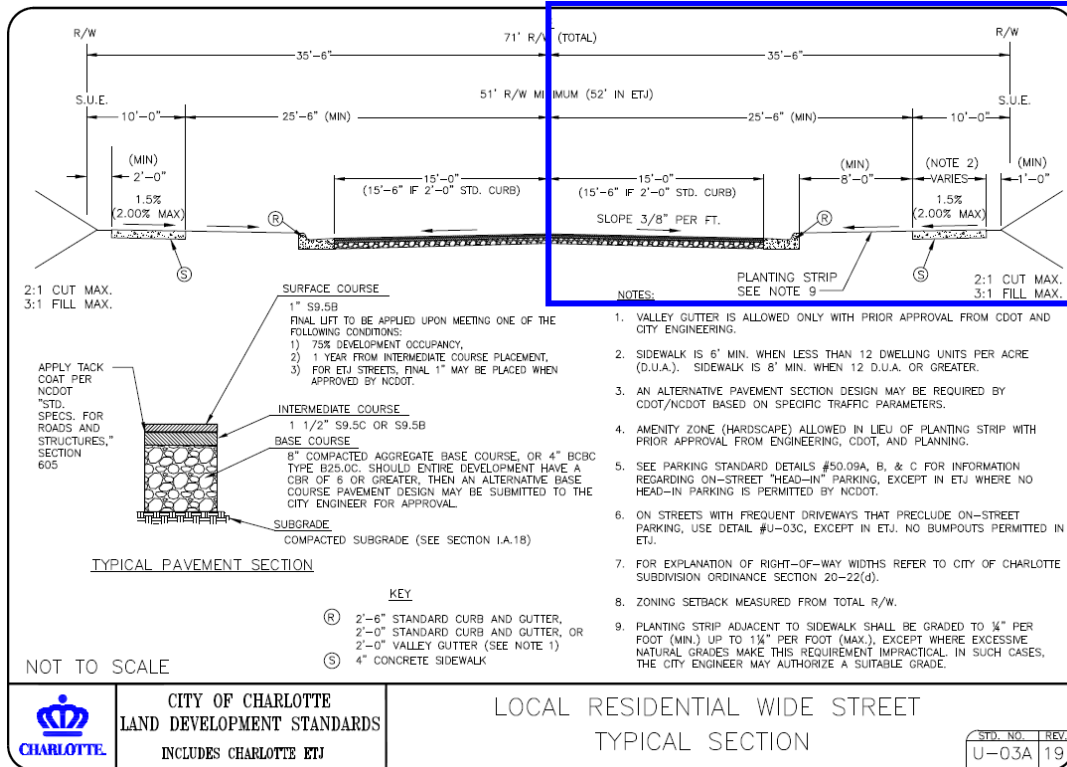


Reference (CLDSM standard detail U-02) for street typical.

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- c. **Alleyway/Paper Right of Way:** Construct a CLDSM standard detail number *U-03 – Local Residential Wide Street Typical Section* (for the portion highlighted in blue) along the property frontage of alleyway/paper right of way. 8-foot planting strip and 8-foot sidewalk is only required along this rezoning site’s property frontage since DUA is greater than 12.

Reference (CLDSM standard detail U-03) for street typical.



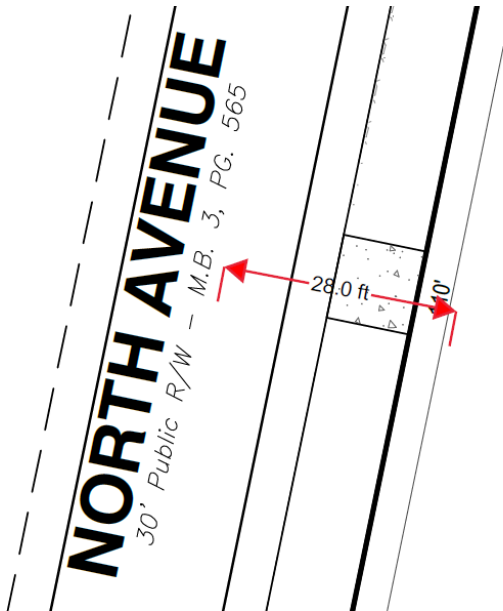
Label and dimension the curb and gutter from the centerline for each road on the site plan.

2. Traffic Study:

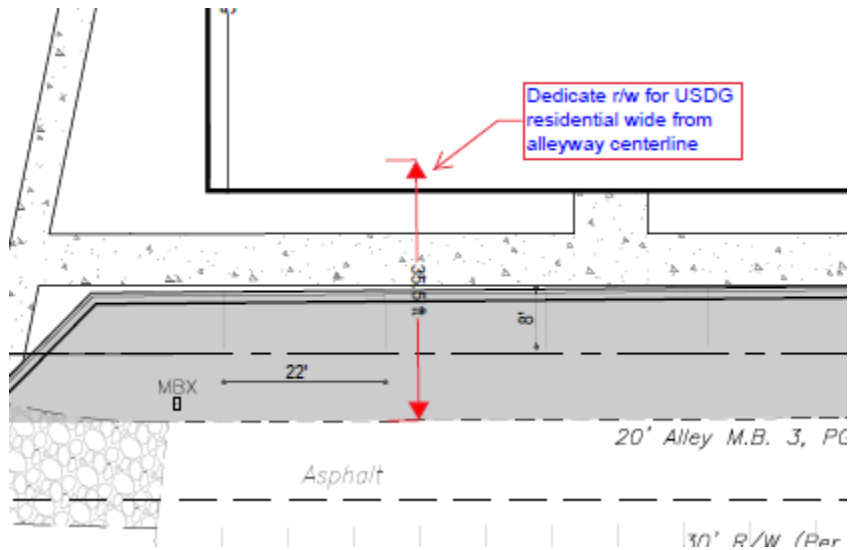
A Traffic Impact Study (TIS) is not necessary for the complete review of this petition due to the site generating less than 2,500 daily trips. If during the permitting process the site generates more than 2,500 daily trips, then a traffic study will be required.

3. Revise site plan and conditional note(s) to commit to dedicate 28-feet of public right-of-way from the North Avenue and B Avenue road centerlines. The site plan shall label and dimension the right-of-way from the road centerline, per CLDSM standard detail number *U-02 – Local Residential Medium Street Typical Section*.

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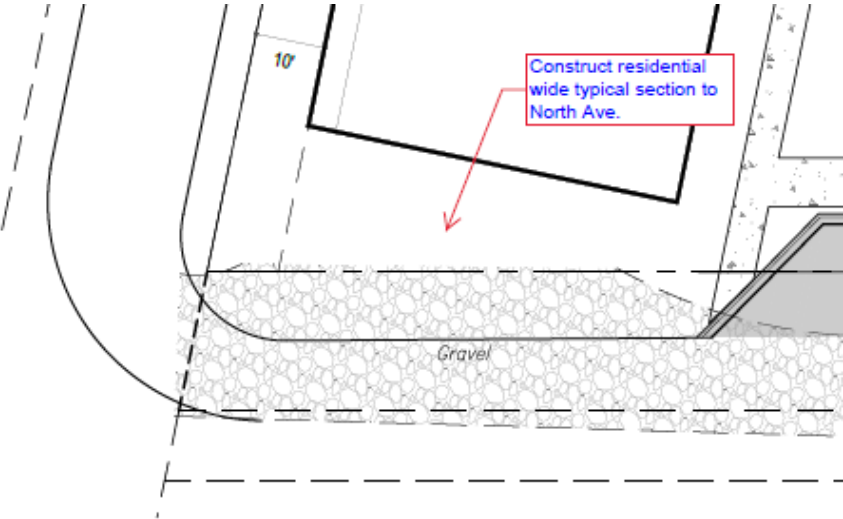
4. Revise site plan and conditional note(s) to commit to dedicate 35.5-feet of public right-of-way from the alleyway centerline, if public right of way is already platted. The site plan shall label and dimension the right-of-way from the road centerline, per CLDSM standard detail number U-03 – *Local Residential Wide Street Typical Section*. If this is confirmed to only be a 20-foot wide alleyway, coordinate with CDOT to determine the appropriate requirements.



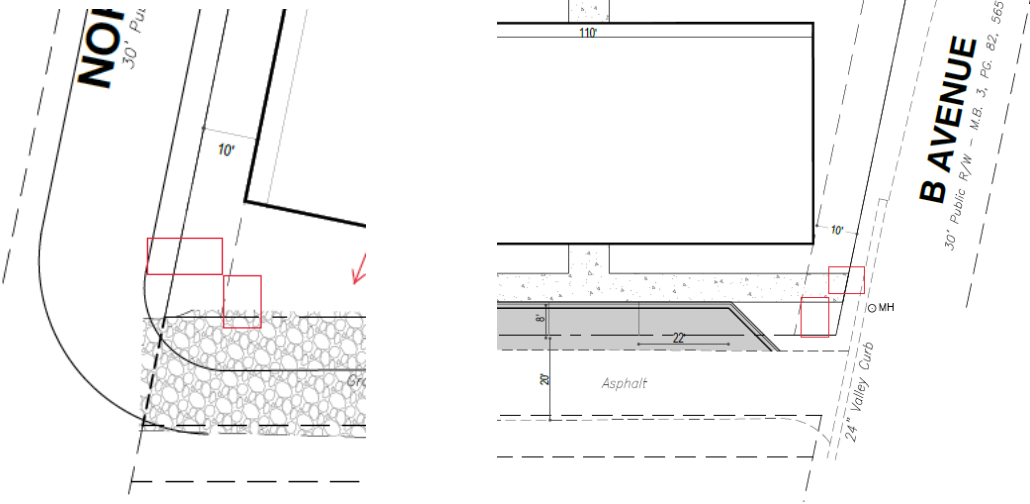
5. Revise site plan and conditional note(s) to commit to construct an 8-foot planting strip and 8-foot sidewalk on all public street frontages per Chapter 19 / Chapter 20 of the City Code and the Charlotte WALKS Council-adopted policy. 8-foot sidewalk is needed since more than 12 dwelling units per acre is proposed. The site plan shall label and dimension both items from the back of curb and gutter and road centerline.

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- 6. Revise site plan and conditional note(s), including Site Plan Conditional Note 3.a, Access, Transportation, and Improvements to extend the Residential Wide typical section on the alleyway / paper right of way to North Avenue.



- 7. Revise site plan and conditional note(s) to commit to construct two PROWAG ramps at the alleyway / paper right of way intersections with North Avenue and B Avenue.



- 8. A site plan note(s) specifying dedication and fee simple conveyance of all rights-of-way to the City before the site's first building certificate of occupancy is issued is needed. CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible.

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

- 1. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
- 2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the

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entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.

3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.
7. The petitioner needs to complete and submit the [Right of way Abandonment Petition form](#) to CDOT for review. The decision of this rezoning petition does not correlate with the decision of the possible abandonment, as this decision is issued within the separate Right of Way Abandonment process that is controlled by North Carolina General Statutes.
8. To attain a street and pedestrian lighting recommendation, the petitioner should coordinate with Anthony Mendez, CDOT. Please visit the following link:
<https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>