

Rezoning Transportation Analysis
Petition Number: Insert Zoning Petition #2021-012
General Location Identifier: Tax ID 10304112

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Revision Log:	Date	Description
	02-25-2021	First Review (WB)
	03-25-2021	Second Review (WB)

General Review Information

The petition is located on Wilora Lake Road (City-maintained, Minor Collector) and Hollyfield Drive (City-maintained, Minor Collector). The petition is in the East Wedge and is outside Route 4. Applicable area plans include the Eastland Area Plan.

Active Projects Near the Site:

- Wilora Lake Minor Gap Sidewalk Project
 - Queued for construction
 - General Services PM: Becky Chambers

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located on two City-maintained, minor collector roads, Wilora Lake Road and Hollyfield Drive. The site abuts the northern limits of the Eastland Mall TIS study area. The site plan commits to constructing an 8-foot planting strip and an 8-foot sidewalk along the site's Wilora Lake Road frontage leaving a sidewalk gap of less than 100' north of the site. Site plan revisions are needed to meet ordinance requirements and/or the outstanding items including, but not limited to constructing curb and gutter along the site's Wilora Lake Road frontage at 17.5-feet from the centerline per Chapter 20 (Subdivision ordinance) and extending the proposed construction of an 8-foot planting strip and 8-foot sidewalk northward and beyond the site's frontage to the Hollyfield Drive intersection. The sidewalk extension will provide pedestrian connectivity and accessibility in accordance with Council-Approved Charlotte Walks Plan and help CDOT close a gap of less than 100' in the pedestrian infrastructure of the area. Further details are listed below.

Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source

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Existing Use	Single Family	1 Dwelling	10	Tax Record
Entitlement with Current Zoning	Single Family (R-4 4.57 acres)	4 Dwellings	215	General Guidance from Planning
Proposed Zoning	Townhomes	89 Units	635	Site Plan: 11-30-20

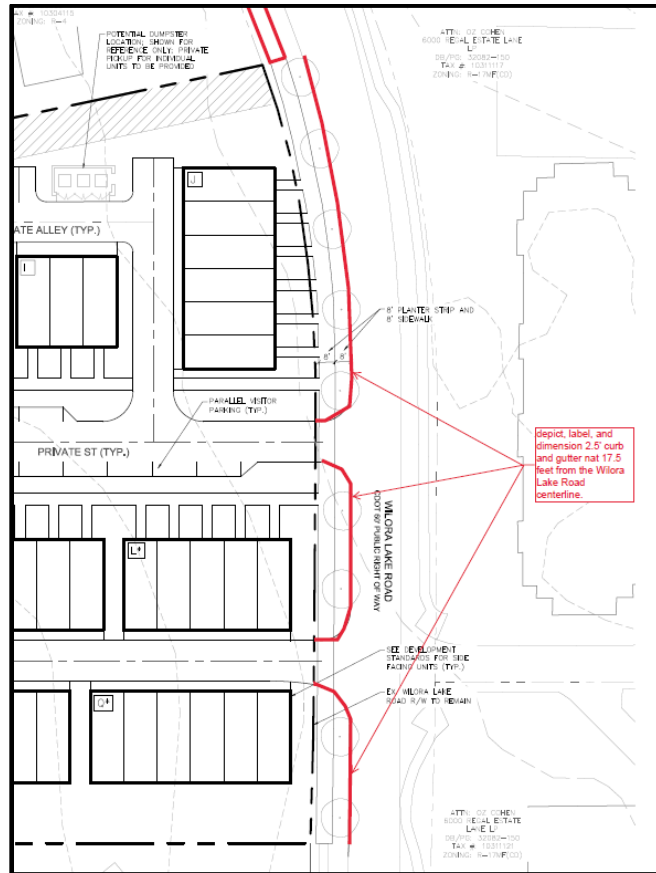
Provide comments to the specified comments below.

Outstanding Issues

Strikethrough = Resolved

1. **Curblines:** The proposed zoning district has a setback measured from back of the existing or proposed future curblines.
 - a. **Wilora Lake Road:** The curb and gutter needs to be at 17.5 feet from the Wilora Lake Road centerline.

Label and dimension the back of curb and gutter from the road centerline.



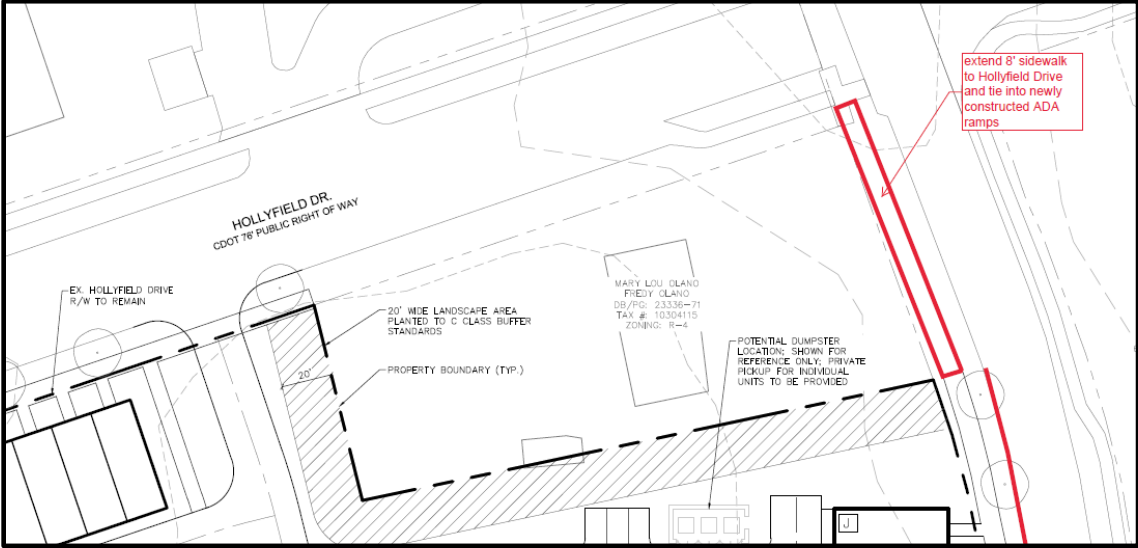
- b. ~~Hollyfield Drive:~~ The curb and gutter is at it future location. No curb relocation needed.

2. Traffic Study: (Choose One)

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A Traffic Impact Study (TIS) is not necessary for the complete review of this petition due to the site generating less than 2,500 daily trips. If during the permitting process the site generates more than 2,500 daily trips, then a traffic study will be required.

- 3. The site plan commits to construct an 8-foot planting strip, 8-foot sidewalk along the site's Wilora Lake Road frontage leaving a sidewalk gap of less than 100' north of the site. Revise site plan and conditional note(s) to commit to extend the proposed 8-foot planting strip and 8-foot sidewalk to the Hollyfield drive intersection and tie into the newly constructed ADA curb ramps to assist CDOT in close a gap in pedestrian infrastructure.



The site plan shall label and dimension both items from the back of curb and gutter.

- ~~4. Add a site plan note specifying dedication and fee simple conveyance of all rights-of-way to the City before the site's first building certificate of occupancy is issued is needed. CDOT requests rights-of-way set at 2' behind back of sidewalk where feasible. Rescinded (3/22/2021)~~
- ~~5. Add a site plan note specifying that all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued is needed. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes. Rescinded (3/22/2021)~~

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. These requirements shall be reflected on the rezoning plan, as appropriate.

- 1. The setback for this district is measured from the back of the existing or future curbline as determined by CDOT and Planning during the permitting process.
- 2. CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte, per the City of Charlotte's Driveway Regulations.
- 3. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on NCDOT-maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs shall not interfere with sight distance at the entrance(s) and shall be identified on the site plan.

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4. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and NCDOT on NCDOT-maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
5. All proposed commercial driveway connections to a future public street will require a driveway permit submitted to CDOT for review and approval.
6. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
7. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City-maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to construction/installation. Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.
8. To attain a street and pedestrian lighting recommendation, the petitioner should coordinate with Anthony Mendez, CDOT. Please visit the following link:
<https://charlottenc.gov/Transportation/Programs/Pages/StreetLighting.aspx>