

Rezoning Transportation Analysis

Petition Number: 2020-088

General Location Identifier: Tax ID 02932106A

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Revision Log:

Date	Description
07-23-2020	First Review

General Review Information

The site is on Johnston Oehler Road (major thoroughfare, city maintained) and Prosperity Church Road (major thoroughfare, city maintained) and is in a wedge outside Route 4. The site is within the limits of the Prosperity Hucks Area Plan.

Active Projects Near the Site:

- Project Name
 - There are no active projects within the immediate project area.

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located on two major thoroughfare road. The petitioner should revise the site plan to meet the ordinance requirements and the outstanding items listed below.

Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Single Family	3 dwellings	30	Tax Record
Entitlement with Current Zoning	Single Family	260 dwellings	2,500	General Guidance from Planning and RZ 2006-126
Proposed Zoning	Shopping Center High-Turnover Sit-Down Restaurant	26,900 sf 5,300 sf	2,462 595	Site Plan: 05-15-20

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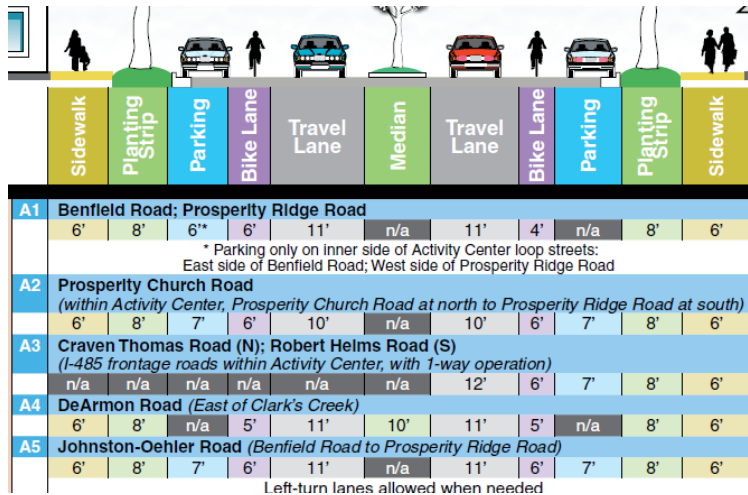
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Fast Food Restaurant with Drive-Thru Window	2,500 sf	1,177
Gasoline with Convenience Market	14 vfp	2,875
Medical-Dental Office Building	10,000 sf	348
General Office Building	10,000 sf	114
Health/Fitness Club	70,000 sf	3,340
Day Care Center	5,300 sf	252
Multifamily Housing Mid-Rise - (Apartments)	320 du	1,742
Multifamily Housing Low-Rise - (Townhomes)	107 du	768

Please provide responses to our comments.

Outstanding Issues ~~Strikeout~~ = Not an outstanding issue

1. **Curbline** The proposed zoning district has a setback measured from back of the existing or proposed future curblines.
 - a. **Johnston Oehler Drive:** Location of curb and gutter should be placed per A5 below.
 - b. **Prosperity Church Road:** Location of curb and gutter should be placed per A1 below.



The site plan should show the curb and gutter labeled and dimensioned from the centerline for each road.

2. **Traffic Study** A Traffic Impact Study is necessary for the complete review of this petition due to the site generation of more than 2500 daily trips.

Staff has received the TIS submittal and will continue to review prior to scheduling the public hearing. This will allow for the minimum time necessary for CDOT to review and approve the study and reach agreement with the petitioner on the required transportation commitments, and have them included

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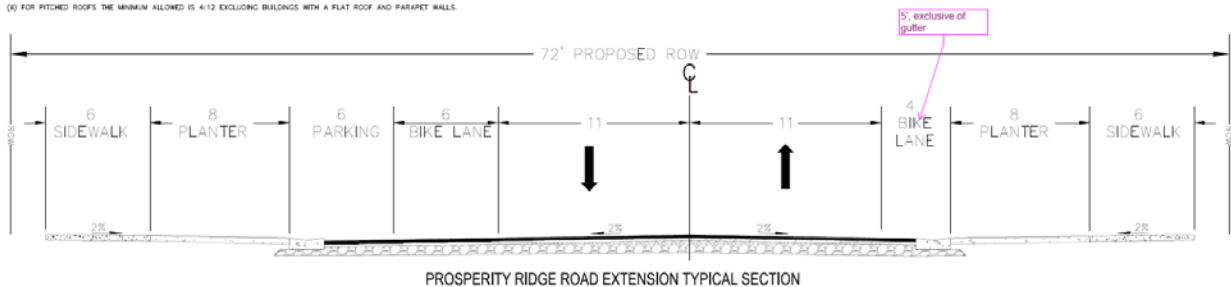
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on the last site plan submitted no fewer than 4 weeks prior to the public hearing. Therefore, additional traffic mitigation comments may be forthcoming once the TIS is approved by CDOT and NCDOT.

3. In accordance with the Prosperity Hucks Area Plan road cross sections, the petitioner should revise the site plan and conditional note(s) to commit to dedicate right-of-way from the Johnston Oehler Drive and Prosperity Church Road centerline. The site plan should label and dimension the right-of-way from the road centerline. Please provide 2 feet of right of way from back of sidewalk where feasible.
4. The petitioner should revise the site plan and conditional note(s) to clarify which internal streets are proposed as public or private streets.
5. The petitioner should revise the site plan and conditional note(s) to clarify which USDG street sections are proposed on each internal street.
6. The petitioner should revise the site plan and conditional notes (Section 3.C. and 5.D.) to include either a planting strip or amenity zone adjacent to on-street parking, in conformance with the Prosperity Hucks Area Plan cross sections.
7. The petitioner should revise the site plan and conditional note(s) to revise the Prosperity Ride Road Extension to replace the proposed 4-foot bike lane with a 5-foot bike lane. Please revise the proposed valley gutter shown to a 2.5-foot curb and gutter.

(9) FOR PITCHED ROOFS THE MINIMUM ALLOWED IS 4:12 EXCLUDING BUILDINGS WITH A FLAT ROOF AND PARAPET WALLS.



8. The petitioner should revise the site plan and conditional note(s) to include the recommended improvements, as included within the TIS.
9. The petitioner should revise the site plan and conditional note (Section 3.E.) specifying all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued. Alternatively, the petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes or as listed within the approved TIS.

~~10. Resolved Sample format for comment that is resolved~~

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

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1. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on North Carolina Department of Transportation on NCDOT maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrance(s). Such items should be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and the North Carolina Department of Transportation on NCDOT maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.