

Rezoning Transportation Analysis

Petition Number: 2020-025

General Location Identifier: 04124204, 04124204, 04124201, portion of 04124202 & 04124209

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Revision Log:

Date	Description
02-15-2020	First Review
03-25-2020	Second Review

General Review Information

This site has borders along Statesville Avenue (Major Thoroughfare), Sunset Road (Major Thoroughfare), and Milhaven Lane (Minor Collector). The site is located in a corridor outside Route 4 and within the Westside Strategy Area Plan.

Active Projects Near the Site:

- Project Name
 - N/A

CDOT's review of this rezoning petition is intended to ensure consistency with the Transportation Action Plan (TAP) which seeks to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located along a major thoroughfare. The is committing to provide the required 8-foot planting strip, 6-foot sidewalk and curb and gutter along Milhaven Lane. In addition, the petitioner will provide a new public street connection from Milhaven Lane to Statesville Road to improve overall street connectivity for the overall network. The new public street will meet the City's Urban Street Design Guidelines. CDOT is requesting the petitioner to modify the striping on Milhaven Lane to provide additional storage capacity for left-turn lane at Sunset Road.

Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Vacant	NA	0	Tax Record
Entitlement with Current Zoning	Retail	218,400 sf	10,230	RZ 2006-157

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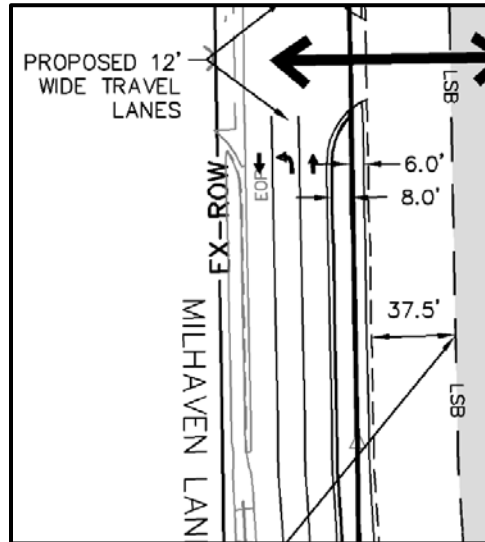
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Proposed Zoning	Warehouse	475,000 sf	800	Site Plan: 01-03-20
Proposed Zoning	Warehouse	450,000 sf	760	Site Plan: 03-16-20

Outstanding Issues

Strikeout = Not an outstanding issue

- ~~1. Millhaven Lane is an existing collector road and should provide the typical for a collector road. The petitioner should revise the site plan and conditional note(s) to place the curb and gutter along Millhaven Lane at a consistent 3-lane section (see below) across the property frontage without tapering the curb and gutter.~~



- ~~2. **Traffic Study** A Traffic Impact Study is not necessary for the complete review of this petition. If during the permitting process the site generates more than 2500 daily trips, then a traffic study will be required.~~
- ~~3. The petitioner should revise the site plan and conditional note(s) to provide additional storage for the existing left-turn storage at Millhaven Lane onto Sunset Road. The existing left-turn lane is only 97' long and the proposed traffic will generate truck traffic that will utilize the capacity.~~
- ~~4. The petitioner should revise the site plan and conditional note(s) to add a left turn on Statesville Road to site aligned across from Keith Drive.~~

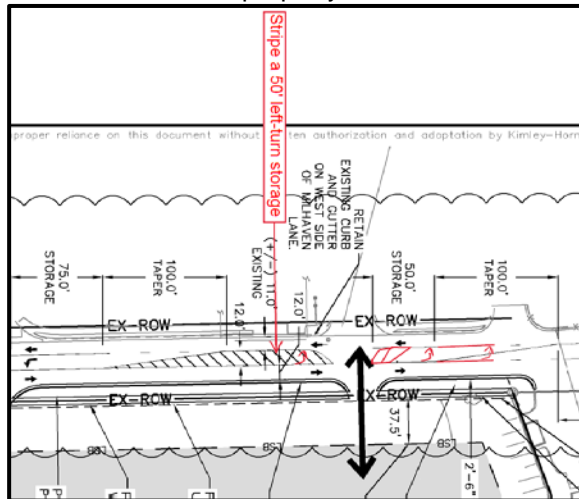
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5. Technical Correction: The petitioner should revise the site plan and conditional note(s) to remove the northern driveway on Millhaven Lane or restrict access to exit only to avoid conflict points with the access across the street.
 - a. **Technical Correction Based on Revised Site Plan** The petitioner should revise the site plan and conditional notes to restripe Millhaven Lane as shown on the exhibit below, in order to keep the proposed full access near the norther property line.



6. The petitioner should revise the site plan to add a note specifying dedication and fee simple conveyance of all rights of way to the City before the site's first building certificate of occupancy is issued. CDOT requests right of way set at 2' behind back of sidewalk where feasible.
7. The petitioner should revise the site plan to add a note specifying all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes.
8. **Resolved** Sample format for comment that is resolved

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Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. The setback for this district is measured from the back of the existing or future curbline as determined by CDOT and Planning during the permitting process.
2. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
3. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on North Carolina Department of Transportation on NCDOT maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrance(s). Such items should be identified on the site plan.
4. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and the North Carolina Department of Transportation on NCDOT maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
5. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
6. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
7. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.