

Rezoning Transportation Analysis

Petition Number: 2020-018

General Location Identifier: Tax ID 14501314

From: Felix Obregon, PE

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Reviewer: Eric Lemieux, PE

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Revision Log:

Date	Description
03-19-20	First Review

General Review Information

The site is on Baltimore Avenue (local street, city maintained) and is in a corridor inside Route 4. The site is within the limits of the New Bern Station Area Plan and Center City 2020 Vision Plan. In reference to the Vision Zero Action Plan, Baltimore Avenue intersects with Remount Road (major thoroughfare, city maintained), which is classified under Charlotte's High Injury Network.

Active Projects Near the Site:

- South Tryon Corridor Implementation
 - The project will implement pedestrian hybrid beacons, bulb-outs, and pavement markings at various locations along South Tryon Street between Clanton Road and Carson Boulevard.
 - Construction: TBD; real estate acquisition to finish end Q1 2020
- I-77 Lane Widening (I-5718A STIP)
 - This project will implement additional managed lanes to widen to a 10-lane freeway from the South Carolina state line to I-277/US 74 (Belk Freeway). The project will also reconstruct the I-77/I-277 (Belk Freeway) interchange, and install ramp meters.
 - Construction: 2029; real estate acquisition to start 2025

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located on a local road. The petitioner should revise the site plan to meet the ordinance requirements and the outstanding items listed below. With consideration to the Vision Zero Action Plan and Charlotte WALKS, CDOT requests pedestrian improvements along Baltimore Avenue to Remount Road, which is classified within Charlotte's High Injury Network. The petitioner's site is located approximately 2/3rd of a mile from the LYNX New Bern Station.

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Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Religious Institution	4,657 SF	50	Tax Record
Entitlement with Current Zoning	Apartments (R22MF .92 acres)	20 DU	105	General Guidance from Planning
Proposed Zoning	Townhomes	20 DU	110	Site Plan: 12-20-19

Please provide responses to comments with plan submittal.

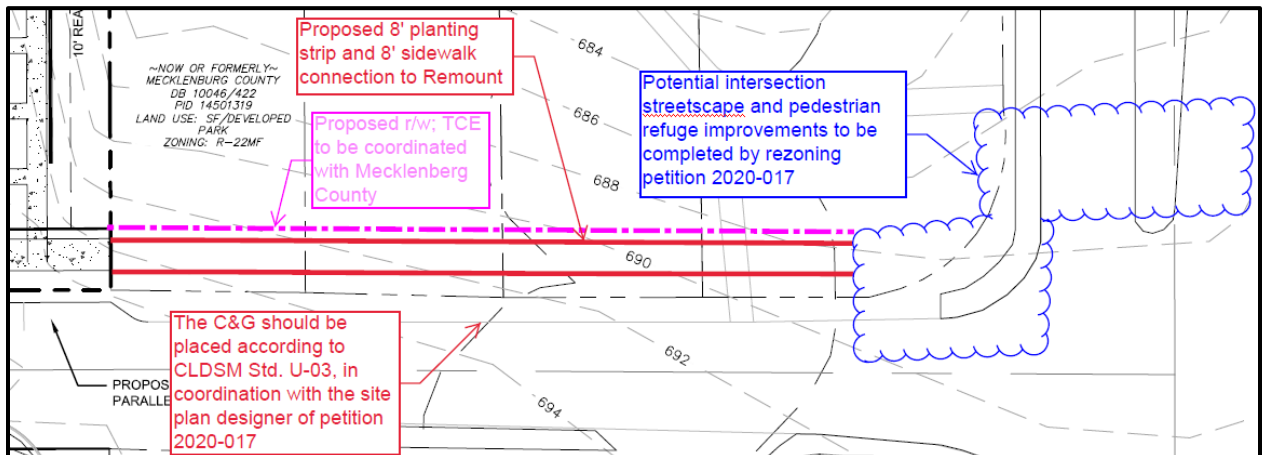
Outstanding Issues

Strikeout = Not an outstanding issue

- 1. Curbline** The proposed zoning district has a setback measured from back of the existing or proposed future curbline.

 - a. Baltimore Avenue:** The proposed location of curb and gutter should be revised to 17.5 feet from existing road centerline, per the Urban Street Design Guidelines (USDG) Charlotte Land Development Standards Manual (CLDSM) standard detail U-03A – Local Residential Wide Street Typical Section and Chapter 19 of the Charlotte City Code. Curb and gutter should be constructed with on-street parking.

The site plan should show the curb and gutter labeled and dimensioned from the centerline for each road.
- ~~2. **Traffic Study** A Traffic Impact Study is not necessary for the complete review of this petition due to the site generation more than 2500 daily trips. If the during the permitting process the site generates more than 2500 daily trips, then a traffic study will be required.~~
3. We request the petitioner to revise the site plan and conditional note(s) to construct a sidewalk connection to Remount Road with curb and gutter, in reference to the Vision Zero Action Plan and Charlotte WALKS. CDOT has notified Mecklenburg County of the possibility of the petitioner possibly attaining right of way and a construction easement to establish this connection. Please contact Jacqueline McNeil at (Jacqueline.McNeil@mecklenburgcountync.gov) or (+1-980-314-2511) to begin this process. The proposed sidewalk connection will tie-into the proposed improvements requested to rezoning petition 2019-017.

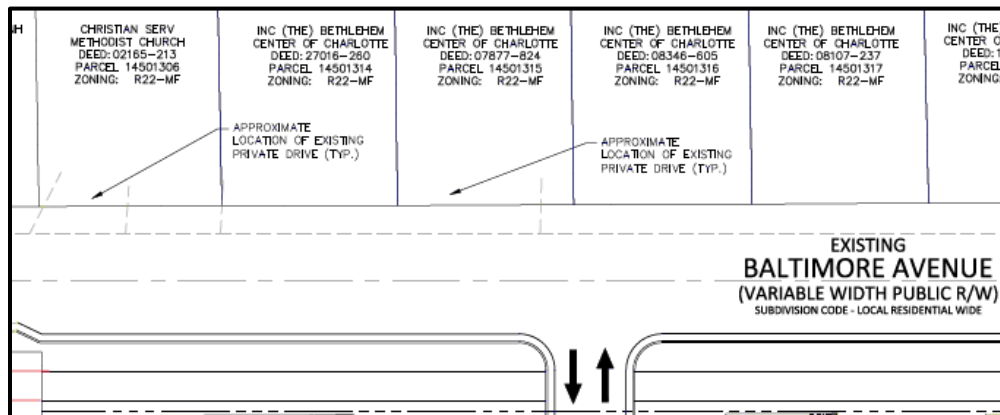
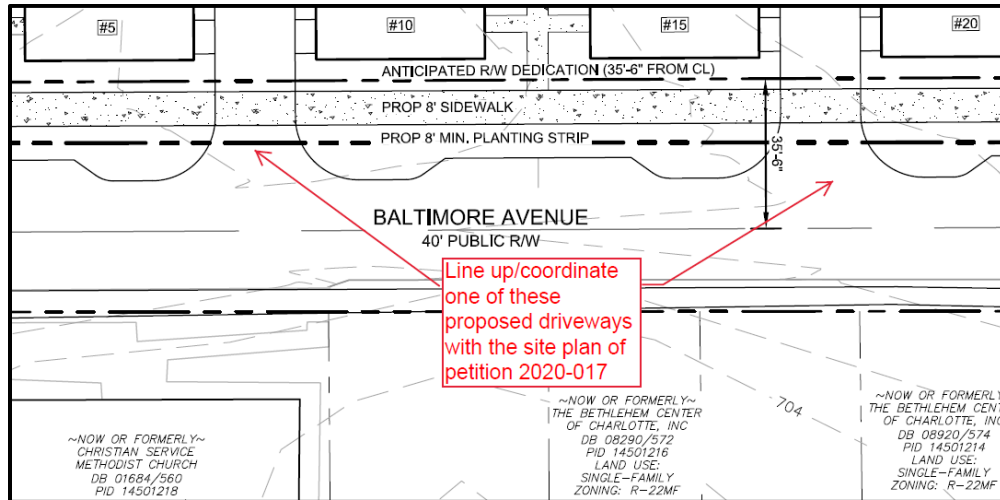


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- The petitioner should revise the site plan and conditional note(s) to coordinate with the petitioner of 2020-017 to align one of the two proposed driveways with the proposed access within rezoning petition 2020-017's proposed site plan, or vice versa.



- The petitioner should revise the site plan to clarify conditional note (Section III. Transportation, 4) to specify dedication and fee simple conveyance of all rights of way to the City before the site's first building certificate of occupancy is issued. CDOT requests right of way set at 2' behind back of sidewalk where feasible.
- The petitioner should revise the site plan to clarify conditional note (Section III. Transportation, 5) to specify that all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes.
- The petitioner should revise the site plan and conditional note(s) to show turning movements of how trucks will handle waste without maneuvering or reversing into the right of way.
- ~~Resolved-Sample format for comment that is resolved~~

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Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on North Carolina Department of Transportation on NCDOT maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrance(s). Such items should be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and the North Carolina Department of Transportation on NCDOT maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.