

Rezoning Transportation Analysis

Petition Number: 2020-007

General Location Identifier: Tax ID 11123334

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Reviewer: Walta Blackmon
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Revision Log:

Date	Description
03-24-20	First Review
06-29-20	Second Review

General Review Information

The site is in the ETJ, on Albemarle Road (major thoroughfare, NCDOT maintained), Rocky River Church Rd (Local Road) and Woodland Beaver Road (Local Road). The site is a Wedge located outside of Route 4 within the project limits. The site is within the limits of the Albemarle Road/I-485 Interchange Study and the Eastside Strategy Plan Study Area.

Active Projects Near the Site:

- There are no active projects within the immediate area; However, the Blair Road Extension Project is an unfunded NCDOT project adjacent to the site. It proposes to construct Blair Road between Albermarle Road and Rocky River Church Road as a two-lane divided (Median or Center Turn Lane) with Bike Facilities and Sidewalks. This project is currently on hold.

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located on a major thoroughfare. The petitioner should revise the site plan to meet the ordinance requirements and the outstanding items listed below.

Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Apartments, Gas Station, and Fast Food	300 MF units 4,012 SF Super Convenience	6,950	Tax Record

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		4,170 SF FF w/ Drive-thru		
Entitlement with Current Zoning	B-1 (10.7 acres) R-15 MF (29.4 acres)	300 MF units 4,012 SF Super Convenience 4,170 SF FF w/ Drive-thru 91,818 SF Retail	12,620	Rezoning Petition 1998-42
Proposed Zoning	B-1 (16.3 acres – up to 80,000 SF) R-17 MF (23.8 acres – up to 300 DU)	300 MF units 4,012 SF Super Convenience 4,170 SF FF w/ Drive-thru 71,818 SF Retail	11,750	Site Plan: 01-15-20

Outstanding Issues

Strikeout = Not an outstanding issue

1. ~~**Curbline**~~ The proposed zoning district has a setback measured from back of the existing or proposed future curbline.
 - a. ~~**Albemarle Road:**~~ The site plan reflects the future location of curb and gutter.
 - b. ~~**Rocky River Church Road Name:**~~ The future location of curb and gutter is in its existing location.
 - c. ~~**Woodland Beaver Road:**~~ The future location of curb and gutter is in its existing location.

The site plan should show the curb and gutter labeled and dimensioned from the centerline for each road.

2. ~~**Traffic Study**~~ A Traffic Impact Study is not necessary for the complete review of this petition. This Petition does not seek to increase the square footage entitlements under the approved rezoning plan for Parcel A (1998-042(C)). This Petition seeks to increase the number of uses that may have an accessory drive through window from 1 to 2. due to the site generation more than 2500 daily trips. An updated traffic is necessary for the complete review of the site. NCDOT will also be requiring and updated Traffic Impact Study.

~~Staff will receive the petition and begin work on it, but the public hearing will not be scheduled until the TIS is received. This will allow for the minimum time necessary for CDOT to review and approve the study and reach agreement with the petitioner on the required transportation commitments, and have them included on the last site plan submitted no fewer than 4 weeks prior to the public hearing. Therefore, additional traffic mitigation comments may be forthcoming once the TIS is approved by CDOT and NCDOT.~~

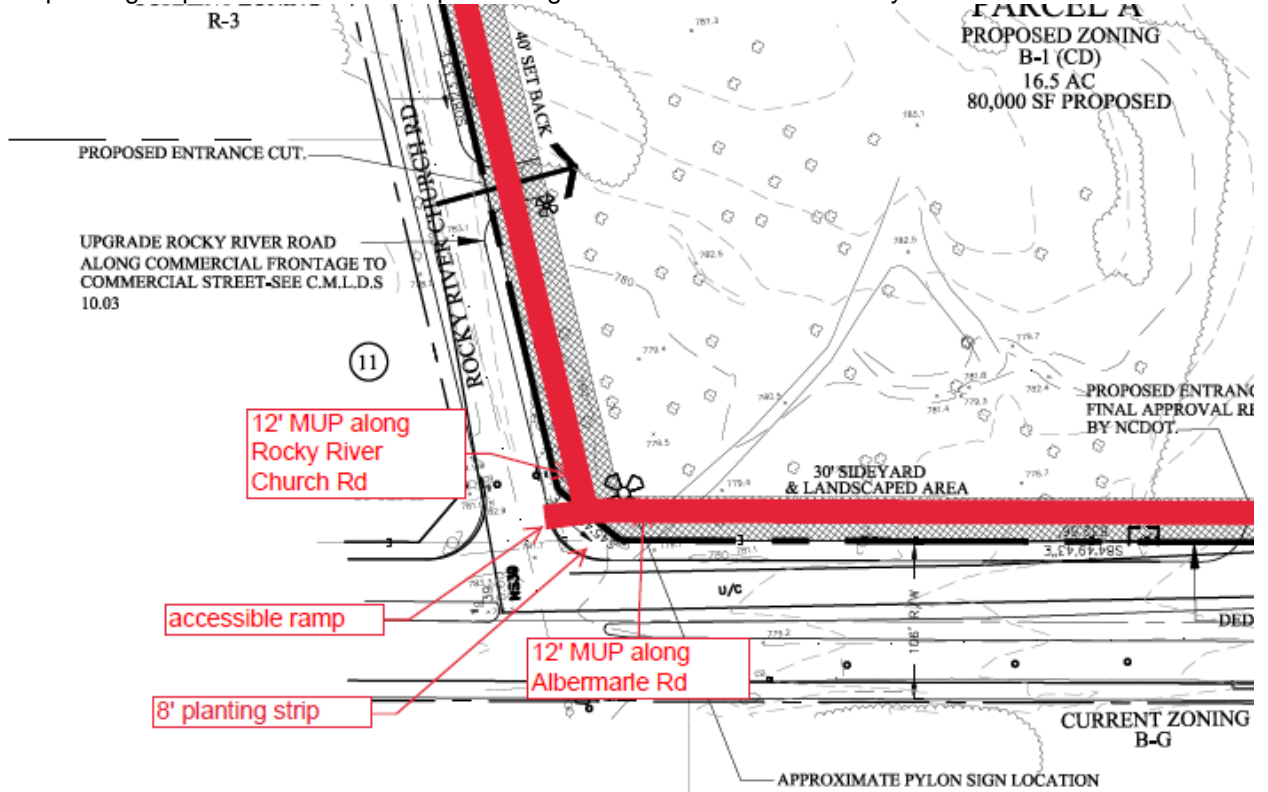
3. The petitioner should revise the site plan and conditional note(s) to commit to dedicate a minimum of 50 feet of right-of-way from the Albemarle Road centerline, per Chapter 12 of the Zoning Ordinance requirements for a Charlotte Regional Transportation Planning Organization (CRTPO) classified major thoroughfare. The site plan should label and dimension the right-of-way from the road centerline.

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4. ~~We request the petitioner to revise the site plan and conditional note(s) to commit to constructing an 8' planting strip and a 12' multi-use path along Albermarle Road and Rocky River Church Road.~~



5. ~~The petitioner should revise the site plan and conditional note(s) to include two ADA curb ramps at each curb return.~~
6. The petitioner should revise the site plan to add a note specifying dedication and fee simple conveyance of all rights of way to the City before the site's first building certificate of occupancy is issued. CDOT requests right of way set at 2' behind back of sidewalk where feasible.
7. ~~The petitioner should revise the site plan to add a note specifying all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued. The petitioner may phase transportation improvements if said improvements and phasing are explicitly described in site plan notes.~~

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. The setback for this district is measured from the back of the existing or future curblines as determined by CDOT and Planning during the permitting process.
2. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
3. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on North Carolina Department of Transportation on NCDOT maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrance(s). Such items should be identified on the site plan.

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4. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and the North Carolina Department of Transportation on NCDOT maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
5. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
6. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
7. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.