

Rezoning Transportation Analysis

Petition Number: 2019-185

General Location Identifier: 06505511

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Reviewer: Eric Lemieux, PE
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Revision Log:

Date	Description
1-16-20	First Review
2-19-20	Second Review (WB)

General Review Information

The site is between Tuckaseegee Road (major thoroughfare, city maintained) and Rogers Street (local, city maintained). The site is in a wedge inside Route 4.

Active Projects Near the Site:

- Tuckaseegee-Berryhill-Thrift Roundabout
 - The project will replace a traffic signal with a roundabout at the five-leg intersection where Tuckaseegee Road and Berryhill Road have 2 legs, and Thrift Road has one leg.
 - Construction date: Starting 3rd Quarter 2020

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located on a major thoroughfare. The site plan commits to an 8-foot planting strip and 6-foot sidewalk on Rogers Street and Tuckaseegee Road. In addition, the site plan is removing multiple driveways on both streets to promote a more walkable community.

Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Church	12,030 sf	80	Tax Record
Entitlement with Current Zoning	Single Family (1.66 ac of R-5)	8 dwellings	80	General Guidance from Planning
Proposed Zoning	Church	5,010 sf	370	Site Plan: 02-10-20

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	Day Care Center	7,020 sf	
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Please provide responses to our comments as part of your resubmittal.

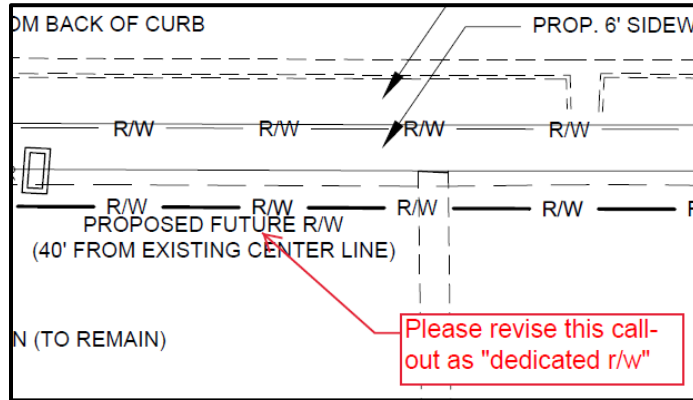
Outstanding Issues

Strikeout = Not an outstanding issue

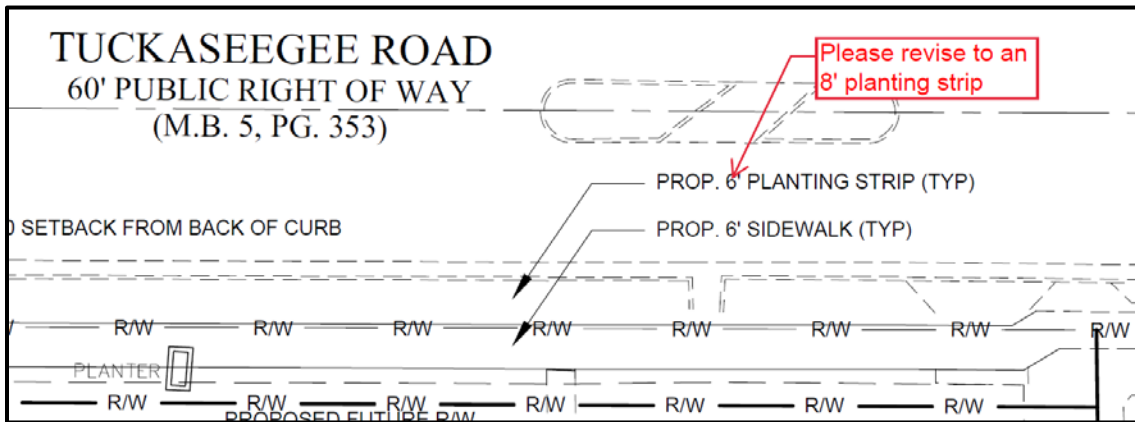
1. ~~**Curbline**~~ The proposed zoning district has a setback measured from back of the existing or proposed future curblines.
 - a. ~~**Tuckaseegee Road**~~: The future location of curb and gutter is in its existing location.
 - b. ~~**Rogers Street**~~: The future location of curb and gutter is in its existing location.

2. ~~**Traffic Study**~~ A Traffic Impact Study is not necessary for the complete review of this petition. If the during the permitting process the site generates more than 2500 daily trips, then a traffic study will be required.

3. The petitioner should revise the site plan and conditional note(s) to commit to dedicate 40 feet of right-of-way from the centerline of Tuckaseegee Road. The site plan should label and dimension the right-of-way from the road centerline.



4. The petitioner should revise the site plan and include a conditional note to commit to construct an 8-foot planting strip and 6-foot sidewalk along Tuckaseegee Road, as required within Chapter 9 of the Zoning Ordinance.

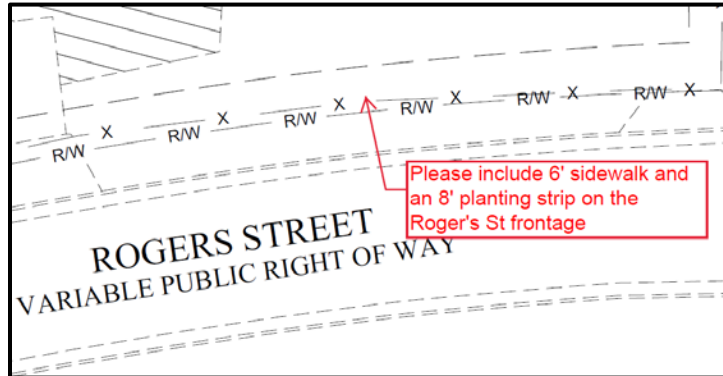


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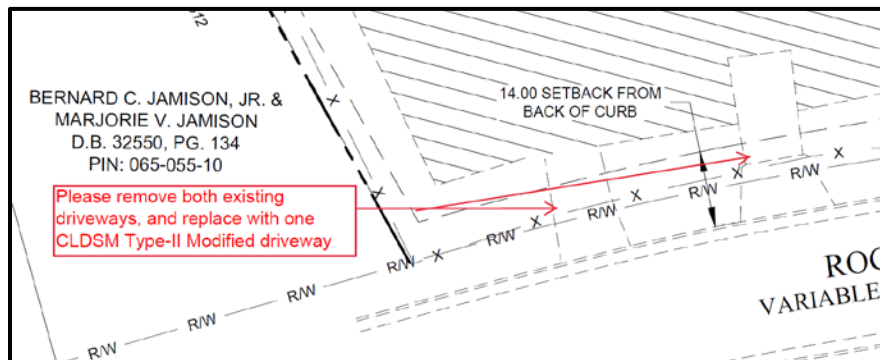
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- The petitioner should ~~revise the site plan and conditional note(s) to commit to construct an 8-foot planting strip and 6 foot sidewalk along Rogers Street, as required within Chapter 9 of the Zoning Ordinance.~~



- The petitioner should ~~revise the site plan and conditional note(s) to remove the existing driveways on Rogers Street and provide one Charlotte Land Development Standards Manual (CLDSM) Type II-modified driveway.~~

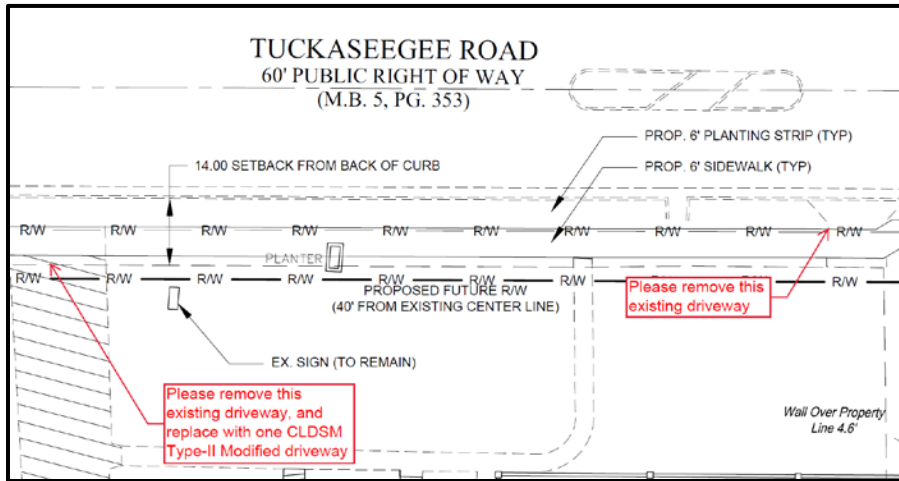


- The petitioner should ~~revise the site plan and conditional note(s) to remove the existing driveways on Tuckaseegee Road and provide one Charlotte Land Development Standards Manual (CLDSM) Type II-modified driveway.~~

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8. ~~The petitioner should revise the site plan to add a note specifying dedication and fee simple conveyance of all rights of way to the City before the site's first building certificate of occupancy is issued. CDOT requests right of way set at 2' behind back of sidewalk where feasible.~~
9. ~~The petitioner should revise the site plan to add a note specifying all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued.~~
10. **Resolved** Sample format for comment that is resolved

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on North Carolina Department of Transportation on NCDOT maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrance(s). Such items should be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and the North Carolina Department of Transportation on NCDOT maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.