

Rezoning Transportation Analysis

Petition Number: 2019-178

General Location Identifier: 06904134, 06904144, 06904135, 06904128, 06904133, 06904132, 06904137, 06904136

From: Felix Obregon, PE
fobregon@charlottenc.gov
704-432-5729

Reviewer: Eric Lemieux, PE
Eric.lemieux@charlottenc.gov
704-336-2683

Revision Log:

Date	Description
1-21-2020	First Review
2-19-2020	Second Review (WB)

General Review Information

The site is generally located between Rozzelles Ferry Road (minor thoroughfare, city maintained) and W Trade Street (minor collector, city maintained). The site is in a wedge inside Route 4.

Active Projects Near the Site:

- Yellowstone-Zebulon Street Connectivity
 - This project will improve access and visibility for Martin Luther King Park and the County's future Stewart Creek Greenway and will encourage private development along Rozzelles Ferry Road and the business park on Zebulon Drive.
 - Construction: TBD; bid phase is in-progress
- Rozzelles PedScope (Rozzelles Ferry / Zebulon to W. Trade) CIP
 - This project will provide new pedestrian refuge medians, new wheelchair ramps, street resurfacing, landscaping, street trees and drainage improvements along Rozzelles Ferry Road.
 - Construction: TBD; bid phase is in-progress
- 5 Points Plaza Improvements CNIP
 - This project will implement pedscape improvements for a new public plaza at the Five Points intersection, including landscaping, pedestrian lighting, etc.
 - Construction: TBD; bid phase is in-progress
- I-77 Express Lanes I-5405 STIP
 - The project will implement high occupancy toll (HOT) lanes and convert existing high occupancy vehicle (HOV) lanes to high occupancy toll (HOT) lanes from I-277 (Brookshire Freeway) to West Catawba Avenue (Exit 28).
 - Construction: active
- NC 16 (Brookshire Boulevard) U-5955 STIP
 - The project will implement a westbound through lane on NC 16 between a point west of Idaho Drive and I-85 and will improve the I-85 northbound ramp to eastbound NC 16.
 - Construction: 2023

CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

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This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

Based on our review of the petition, we offer the following information for your consideration.

Transportation Summary

The site is located on both minor thoroughfare and collector roads. The site plan commits to a planting strip and sidewalk along Zebulon Avenue, Judson Avenue, and West Trade Street to improve pedestrian connectivity. petitioner should revise the site plan to meet the ordinance requirements and the outstanding items listed below.

Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Vacant	NA	0	Tax Record
Entitlement with Current Zoning	Warehouse (11.48 ac of I-2)	172,200 sf	320	RZ 2007-106 and General Guidance from Planning
Proposed Zoning	Townhomes Retail	105 dwellings 12,000 sf	2,180	Site Plan: 02-10-20

Please provide responses to our comments as part of your resubmittal.

Outstanding Issues

Strikeout = Not an outstanding issue

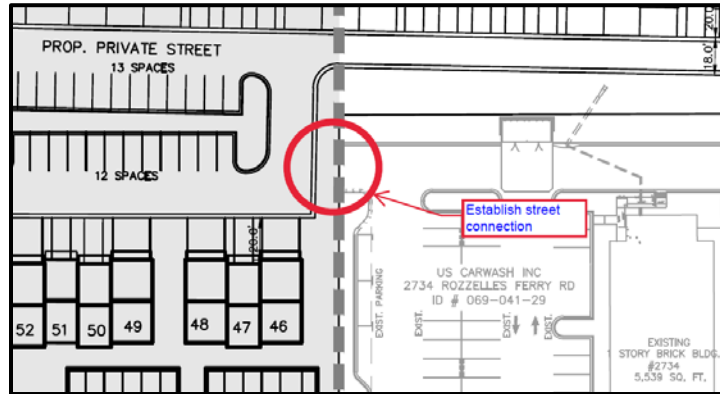
- ~~**Curbline** The proposed zoning district has a setback measured from back of the existing or proposed future curbline.
 - ~~**Rozzelles Ferry Road:** The future location of curb and gutter is in its existing location.~~
 - ~~**Trade Street:** The future location of curb and gutter is in its existing location.~~
 - ~~**Judson Avenue:** The future location of curb and gutter is in its existing location.~~
 - ~~**Zebulon Avenue:** The future location of curb and gutter is in its existing location.~~~~
- ~~**Traffic Study** A Traffic Impact Study is not necessary for the complete review of this petition. If during the permitting process the site generates more than 2,500 daily trips, then a traffic study will be required.~~
- ~~The petitioner should revise the site plan and conditional note(s) to commit to dedicate a minimum of 30 feet of right of way from the existing Rozzelles Ferry Road centerline, per the CRTPO 2004 Mecklenburg-Union Metropolitan Planning Organization (MPO) Thoroughfare Plan and Section 12.103. of the Zoning Ordinance. The site plan should label and dimension the right-of-way from the road centerline.~~
- ~~The petitioner should revise the site plan and conditional note(s) to commit to dedicate a minimum of 36 feet of right of way from the existing Trade Street centerline, per Urban Street Design Guidelines (USDG) standards. The site plan should label and dimension the right-of-way from the road centerline.~~

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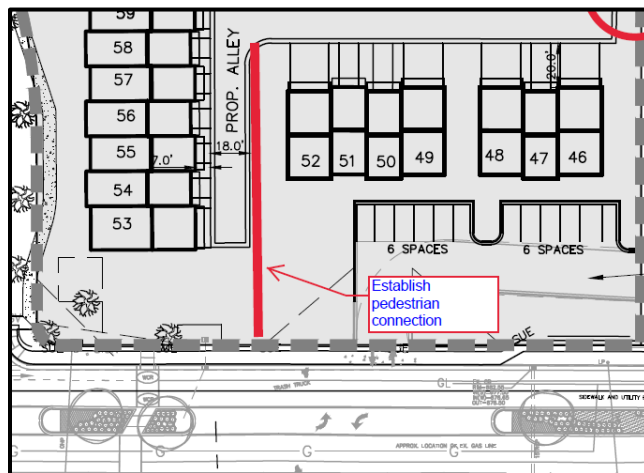
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5. Per Chapter 9 of the Zoning Ordinance for Urban Residential Districts, the petitioner should revise the site plan and conditional note(s) to commit to construct an 8-foot planting strip and 6-foot sidewalk along the frontages of Trade Street, Rozzelles Ferry Road, Zebulon Avenue, and Judson Avenue. Proposed sidewalks may meander around existing trees. Please include call-outs and dimensions of the proposed sidewalk and planting strips.
6. The petitioner should revise the site plan and conditional note(s) to commit to establish a private street connection at the US Carwash Inc for improved internal street connectivity.



7. Per the council-adopted Vision Zero policy, the petitioner should revise the site plan and conditional note(s) to include stop sign and speed limit sign locations.
8. The petitioner should revise the site plan and conditional note(s) to include a pedestrian connection from the proposed alley to Rozzelles Ferry Road to improve pedestrian connectivity and encourage multi-modal transportation mobility.



9. The petitioner should revise the site plan to add a note specifying dedication and fee simple conveyance of all rights of way to the City before the site's first building certificate of occupancy is issued. CDOT requests right of way set at 2' behind back of sidewalk where feasible. Please label/call-out the proposed rights of way on the site plan.
10. The petitioner should revise the site plan to add a note specifying all transportation improvements will be approved and constructed before the site's first building certificate of occupancy is issued.

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11. The petitioner should revise the site plan and conditional note(s) to provide a mid-block crossing for pedestrians crossing Zebulon Avenue. The location of the mid-block location should be located where appropriate sight distance is available.
12. **New Comment based on revised site plan (02-11-2020)** The site plan needs to add a conditional note stating the crosswalks at Zebulon Avenue and Judson Avenue intersection need to be approved by CDOT, prior to the installation of them.
13. ~~Resolved-Sample format for comment that is resolved~~

Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on North Carolina Department of Transportation on NCDOT maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrance(s). Such items should be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and the North Carolina Department of Transportation on NCDOT maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.