

# Rezoning Transportation Analysis

Petition Number: 2019-173

General Location Identifier: 14129102, 14129105, 14118165, 14129101, 14129103, 14129107, 14118119

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## Revision Log:

Date	Description
01-17-2020	First Review
05-20-2020	Second Review (WB)
09-22-2020	Third Review

## General Review Information

The site is at the southern end of Garrison Road (minor thoroughfare, state maintained). The site is in a wedge outside Route 4 and is within the limits of the Dixie Berryhill Strategic Plan.

### Active Projects Near the Site:

- CLT Airport and River District Area Feasibility Study
  - Express Design Feasibility study being done to determine what road improvements are required to sustain recent area growth in CLT airport, River District and surrounding areas

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CDOT's review of this rezoning petition is intended to provide consistency with council adopted policies, plans, standards and guidelines including, but not limited to, the [Transportation Action Plan \(TAP\)](#), [Vision Zero](#), [Urban Street Design Guidelines \(USDG\)](#), [Center, Corridor and Wedges](#), [Charlotte BIKES](#), [Traffic Impact Study Guidelines](#) and [Charlotte WALKS](#). With a commitment to design complete streets, this review is to ensure that the City's transportation network supports current and future land uses and includes streets that provide safe and comfortable mobility for motorists, pedestrians, bicyclists, and transit users.

This document is primarily for communication to Planning Department staff as part of the overall City staff analysis of the rezoning petition and includes an overall summary of the case from a transportation perspective, information on trip generation, and resolved or outstanding site plan concerns. Additional advisory information about the driveway permit process is provided for information only.

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Based on our review of the petition, we offer the following information for your consideration.

## Transportation Summary

The site is located on Garrison Road, a minor thoroughfare, State-maintained road. The latest site plan includes various revisions, which results in several outstanding issues for transportation improvements. The outstanding issues are in accordance of meeting standards, manuals and for the consideration of alternative realignment of Garrison Road for connection to West Boulevard Improvement Design Project. The petitioner should revise the site plan to meet the outstanding items listed below.

## Trip Generation

Scenario	Land Use	Intensity	Trip Generation (vehicle trips/day)	Source
Existing Use	Single Family	6 dwellings	60	Tax Record

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Entitlement with Current Zoning	Single Family (150 ac of R-3)	450 dwellings	4,150	General Guidance from Planning
Proposed Zoning	Warehouse	1,500,000 sf	2,420	Site Plan: 12-23-19
	General Industrial Warehouse	147,000 sf 1,096,500	1,715	Submitted TIS: 04-30-20

Please provide responses to our comments as part of your resubmittal.

## Outstanding Issues

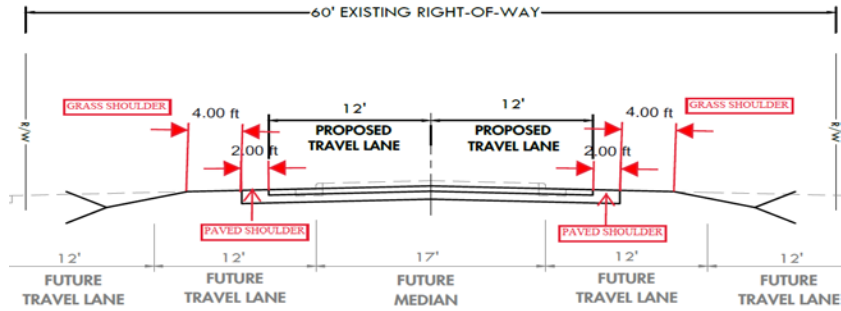
~~Strikeout~~ = Not an outstanding issue

1. ~~Traffic Study:~~ A Traffic Impact Study is necessary for the complete review of this petition. Staff will receive the petition and begin work on it, but the public hearing will not be scheduled until the TIS is received. This will allow for the minimum time necessary for CDOT to review and approve the study and reach agreement with the petitioner on the required transportation commitments and ~~have them included on the last site plan submitted no fewer than 4 weeks prior to the public hearing.~~ Therefore, additional traffic mitigation comments may be forthcoming once the TIS is approved by CDOT and NCDOT.
2. ~~The petitioner should revise the site plan and conditional note(s) to commit to building a 12-foot multi-use path on both sides of Garrison Road where the road runs through the property and across site frontage where property is adjacent. The back of the multi-use path is located 58 feet from centerline of the road.~~
3. ~~The future bridge over Beavordam Creek tributary should account for the future greenway. The petitioner should add a conditional note to commit to coordinate with Mecklenburg County Parks and Recreation when designing the bridge to ensure the greenway can be built in the future.~~
4. ~~The petitioner should revise the site plan and conditional note(s) to describe the proposed phasing of the transportation improvements. The phasing should include the mitigations identified in the traffic impact study.~~
5. ~~The petitioner should continue working with CDOT to determine the alignment of the future Western Parkway. Contact Felix Obregon to discuss the feasibility study that is evaluating the alignment.~~
6. ~~New Comment based on revised site plan 05-11-2020:~~ The petitioner should revise the site plan and conditional notes to commit to construct 2-12' lanes with 2' paved shoulders and 4' grass shoulders. for Section A-A. **Outstanding:** Add a note stating the final details will be determined during the permit review process to include input from other departments including but not limited to CDOT, General Services, Charlotte Fire Department, and NCDOT. Also include that any applicable ordinance requirements in addition to what is shown on proposed cross-sections may be required.

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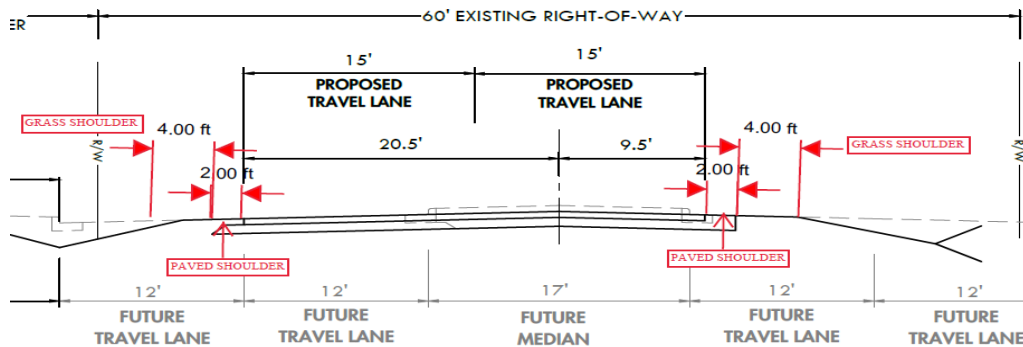
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## SECTION A - A PROPOSED ROAD IMPROVEMENTS (FUTURE BY OTHERS)

7. **Rescinded, see new comment below.** ~~New Comment based on revised site plan 05-11-2020: The petitioner should revise the site plan and conditional notes to commit to construct 2-12' lanes with 2' paved shoulders and 4' grass shoulders for Section B-B.~~

**Outstanding:** Add a note stating the final details will be determined during the permit review process to include input from other departments including but not limited to CDOT, General Services, Charlotte Fire Department, and NCDOT. Also include that any applicable ordinance requirements in addition to what is shown on proposed cross-sections may be required.



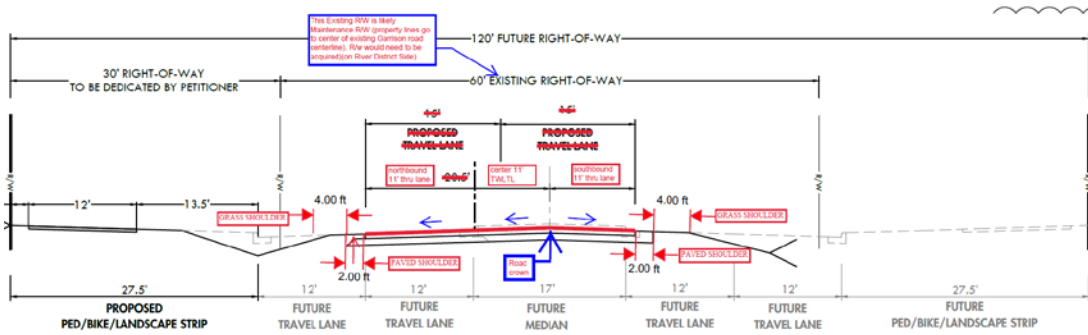
## SECTION B - B PROPOSED ROAD IMPROVEMENTS (FUTURE BY OTHERS)

- a. **New Comment based on revised site plan 08-24-2020:** Revise Section B-B to the redlines below, reflecting the latest coordination between CDOT and NCDOT. A three-lane section with a center two-way left-turn lane will provide median space for left turns into the proposed access points for Development Area A. CDOT encourages the petitioner to consider condensing the number of access points for Development Area A on the along the Realigned Garrison Road.

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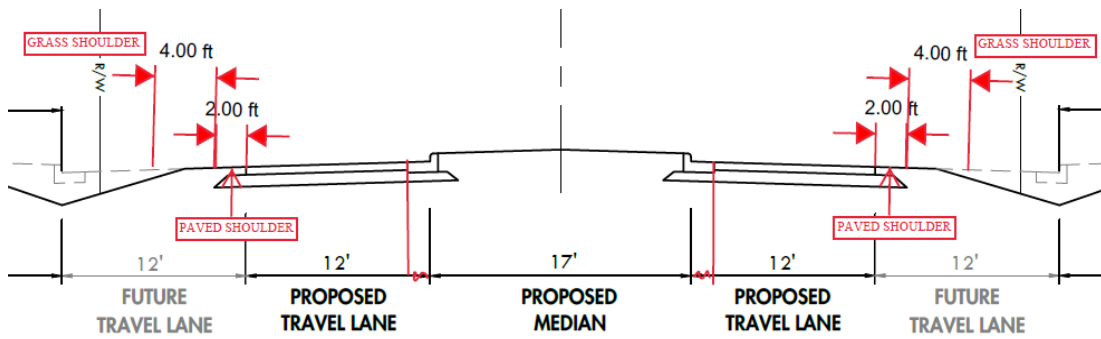
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SECTION B - B

8. ~~New Comment based on revised site plan 05-11-2020:~~ Cross-section C-C depict the general intent of a 3-lane section (2 travel lanes and center turn lane and/or planted median) and a shared-use path for an interim version of future Garrison Road. The petitioner should revise the site plan and conditional notes to commit to construct 2' paved shoulders and 4' grass shoulders.

**Outstanding:** Add a note stating the final details will be determined during the permit review process to include input from other departments including but not limited to CDOT, General Services, Charlotte Fire Department, and NCDOT. Also include that any applicable ordinance requirements in addition to what is shown on proposed cross-sections may be required.



## SECTION C - C

### PROPOSED ROAD IMPROVEMENTS

(FUTURE BY OTHERS)

9. ~~Rescinded New Comment based on revised site plan 05-11-2020:~~ The petitioner should revise the Transportation Improvements and Access Note 3a to the following: The transportation improvements, pedestrian improvements and right-of-way dedication described below in paragraphs 3.a(1), 3.a(2) and 3.a(3) shall be constructed, installed and dedicated by the Petitioner.

10. **New Comment based on revised site plan 08-24-2020:** Revise the site plan and conditional note(s) to clarify the pavement sections that will be constructed by the petitioner, to be consistent with CLDSM standard detail number 11.09. The pavement schedule, as copied from the West Boulevard Extension plans for the Realigned Garrison Road typical cross sections, are provided below for reference.

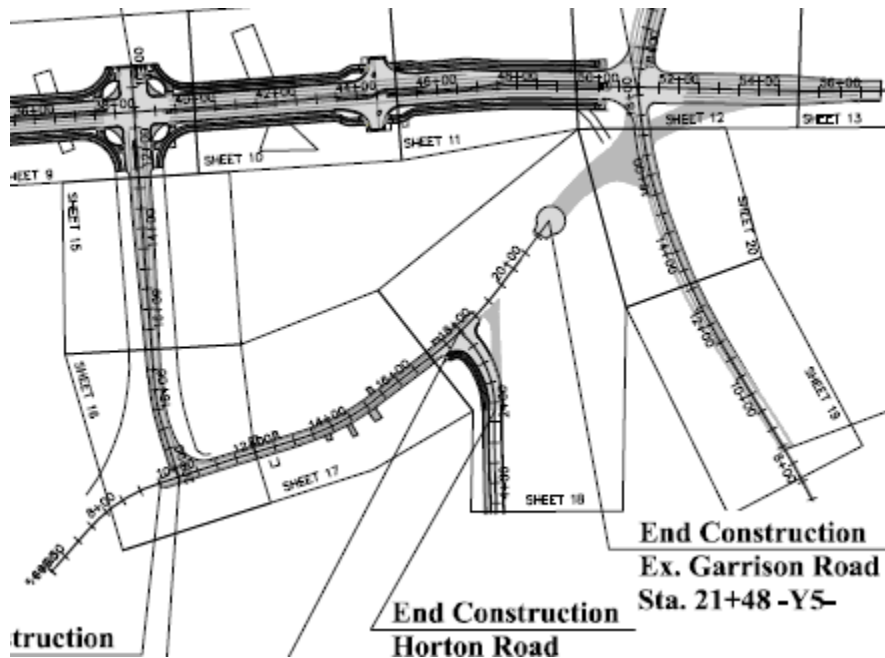
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(C1)	PROP. 3.0 INCHES ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LB/SY IN EACH OF TWO 1.5-INCH LIFTS
(D1)	PROP. 4 INCHES (MIN) ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 456 LB/SY
(E1)	PROP. 8.0 INCHES ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 399 LB/SY IN EACH OF TWO EQUAL LIFTS

11. **New Comment based on revised site plan 08-24-2020:** Revise the site plan's conditional note language to specify the dedication and fee simple conveyance of rights of way within both phases, to the City before the site's first building certificate of occupancy is issued for each respective phase. CDOT requests right of way set at 2' behind back of sidewalk where feasible.
12. **New Comment based on revised site plan 08-24-2020:** Revise the site plan and conditional note(s) to commit to constructing left-turn lanes, with the appropriate storage and bay taper, within the center median at the proposed access points for Development Areas B, C, and D within Phase 1. CDOT encourages the petitioner to align opposing driveways and/or condense the number of proposed access points on Realigned Garrison Road, to minimize the number of left-turn lane bays required.
13. **New Comment based on revised site plan 08-24-2020:** Revise the site plan and conditional note(s) to update the proposed road centerline options of the West Boulevard Improvements and Alternative Garrison Road Improvements to Horton Road, per the latest West Boulevard Improvement design that is shown below.



14. **New Comment based on revised site plan 08-24-2020:** Within the scope of the proposed Phase 2 improvements, revise the road centerline dimensions to comply with AASTHO Green Book minimum centerline radius standards for a major arterial.
15. **New Comment based on revised site plan 08-24-2020:** Revise the site plan and conditional note (Section 3.a.(3).b.ii.) to clarify whether the public right-of-way that is required is for the temporary condition (60-ft) or future condition (120-ft).

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~~16. Resolved Sample format for comment that is resolved~~

## Advisory Information

The following are requirements of the developer that must be satisfied prior to driveway permit approval. We recommend that the petitioner reflect these on the rezoning plan as-appropriate.

1. According to the City of Charlotte's Driveway Regulations, CDOT has the authority to regulate/approve all private street/driveway and public street connections to the right-of-way of a street under the regulatory jurisdiction of the City of Charlotte.
2. Adequate sight triangles must be reserved at the existing/proposed street entrance(s). Two 35' x 35' sight triangles (and two 10' x 70' sight triangles on North Carolina Department of Transportation on NCDOT maintained streets) are required for the entrance(s) to meet requirements. All proposed trees, berms, walls, fences, and/or identification signs must not interfere with sight distance at the entrance(s). Such items should be identified on the site plan.
3. The proposed driveway connection(s) to public streets will require a driveway permit(s) to be submitted to CDOT (and the North Carolina Department of Transportation on NCDOT maintained streets) for review and approval. The exact driveway location(s) and type/width of the driveway(s) will be determined by CDOT during the driveway permit process. The locations of the driveway(s) shown on the site plan are subject to change in order to align with driveway(s) on the opposite side of the street and comply with City Driveway Regulations and the City Tree Ordinance.
4. All proposed commercial driveway connections to a future public street will require a driveway permit to be submitted to CDOT for review and approval.
5. Any fence or wall constructed along or adjacent to any sidewalk or street right-of-way requires a certificate issued by CDOT.
6. A Right-of-Way Encroachment Agreement is required for the installation of any non-standard item(s) (irrigation systems, decorative concrete pavement, brick pavers, etc.) within a proposed/existing City maintained street right-of-way by a private individual, group, business, or homeowner's/business association. An encroachment agreement must be approved by CDOT prior to the construction/installation of the non-standard item(s). Contact CDOT for additional information concerning cost, submittal, and liability insurance coverage requirements.