

SITE DATA

TAX PARCEL NUMBER: 228-124-01
 EXISTING ZONING: B-1 (CD) NEIGHBORHOOD BUSINESS
 PROPOSED ZONING: B-1 (CD) SITE PLAN AMENDMENT
 TOTAL SITE AREA = 11.97 ACRES
 MAXIMUM FLOOR AREA = 95,000 S.F.

PETITIONERS:

BL PATCH & ASSOCIATES, INC.
 2409-B PLANTATION CENTER DRIVE
 MATTHEWS, NC 28105
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 CONTACT: BRANSEN L. PATCH

RALEY MILLER PROPERTIES
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DESIGNER:

SITE SOLUTIONS, INC.
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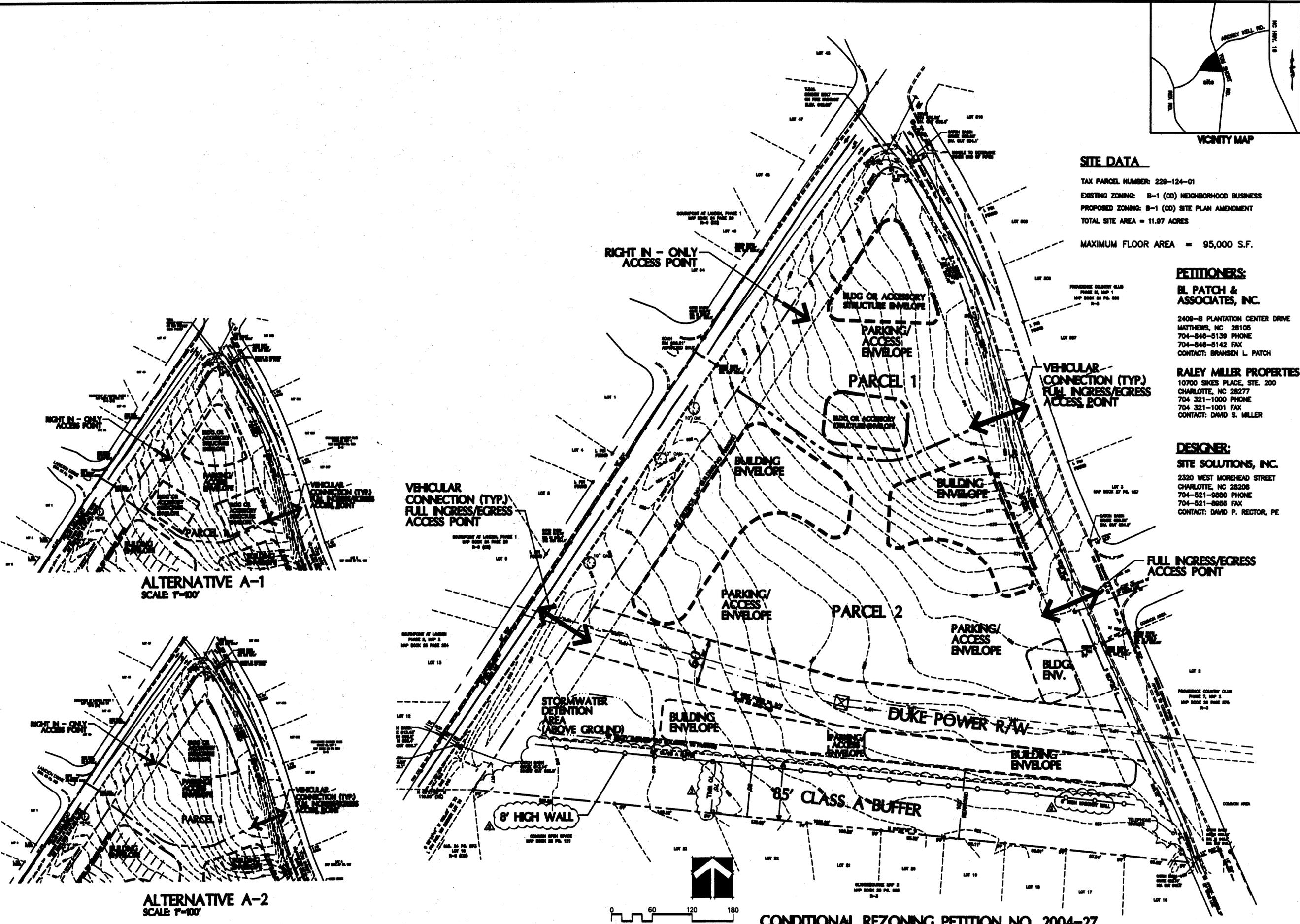
The Fountains at
 Tom Short Road
 BL Patch & Associates, Inc.
 Charlotte, North Carolina

Project No.
Drawn By
Checked By
Approved By
Date

Number of	492	438.04
Public Submittal
Permits per CDPC	7.28.04

Technical
 Data Sheet
 - Alternatives
 (Parcel 1)

RZ-1



General Provisions

1. These Development Standards form a part of the Technical Data Sheet associated with the Rezoning Petition filed by B.L. Patch and Associates, Inc. and Raley Miller Properties to accommodate development of a village type retail/office project to be known tentatively as The Fountains at Tom Short Road on an 11.97 acre site located south of the intersection formed by Ardrey Kell Road and Tom Short Road (the "Site").

2. Development Alternatives A-1 and A-2 for Parcel 1 are schematic representations of development alternatives for Parcel 1, and the approval of this Rezoning Petition by the City of Charlotte City Council will constitute the approval of these Development Alternatives.

3. Subject to the provisions of these Development Standards, the configurations, placements and sizes of the buildings depicted on the Schematic Site Plan, as well as the configurations, sizes and locations of the parking areas, are schematic in nature and may be altered or modified during design development and construction document phases within the building and parking envelopes established on the Technical Data Sheet.

4. Landscape areas, landscape areas, sidewalks and parking areas may be located within the building or accessory structure envelopes and building envelopes depicted on the Technical Data Sheet.

Permitted Uses

1. The Site may be devoted only to the following principal and accessory uses that are permitted by right or under prescribed conditions in the B-1 zoning district.

Uses Permitted By Right

- (1) Bakeries, retail, including manufacturing of goods for sale on premises.
(2) Barber and beauty shops.
(3) Clinics, medical, dental and optical. Notwithstanding the foregoing, any clinic shall be primarily by appointment only, and a 24 hour urgent care facility shall not be permitted on the Site.
(4) Clinics, veterinary.
(5) Cultural facilities.
(6) Dry cleaning and laundry establishments, up to 4,500 square feet on a lot.
(7) Duplex, detached, duplex, triplex or quadplex at a maximum density of 4 dwelling units per acre.
(8) Elementary and secondary schools.
(9) Fences and fence material, retail sales within an enclosed building.
(10) Financial institutions up to 10,000 square feet.
(11) Florist, retail.
(12) Government buildings up to 20,000 square feet.
(13) Health institutions.
(14) Highway and railroad rights of way.
(15) Indoor recreation.
(16) Jewelers, retail.
(17) Laboratories, dental, medical and optical.
(18) Neighborhood food and beverage service.
(19) Nurseries and greenhouse retail and wholesale.
(20) Subject to the square footage limitations set out below under "Maximum Floor Area", Offices, with no single space occupied by a single office use being larger than 10,000 square feet in size.
(21) Outdoor seasonal sales.
(22) Parks, greenways and arboreta.
(23) Post offices.
(24) Printing and publishing, up to 5,000 square feet.
(25) Religious institutions.
(26) Repair or servicing of any article, within an enclosed building, the sale of which is permitted in the district.
(27) Restaurants. Notwithstanding the foregoing, the number of full service, sit down restaurants containing 2,400 square feet or more of space will be limited to 2. However, this limitation will not apply to restaurants less than 2,400 square feet in size.
(28) Subject to the square footage limitations set out below under "Maximum Floor Area", retail establishments, shopping centers and business, personal and recreation services, except for uses permitted only in the B-2 district.
(29) Studios for artists, designers, photographers, musicians, sculptors, gymnasts, potters, wood and leather craftsmen, glass blowers, weavers, silversmiths, and designers of ornamental precious jewelry.
(30) Subdivision sales offices.
(31) Universities, colleges and junior colleges.
(32) A maximum of one vocational school within an enclosed building.

Uses Permitted Under Prescribed Conditions

The following uses shall be permitted on the Site if they meet the standards established in Section 9.015 of the Ordinance and all other requirements of the Ordinance.

- (1) Casinos.
(2) Childcare centers.
(3) Drive-in, motor use.
(4) Gas and service, provided that they are associated with and part of a full service, sit down restaurant.
(5) Nursing homes, rest homes and homes for the aged provided that any such use may be located only in a 1 story structure.
(6) Open space recreational use.
(7) Outdoor recreation.
(8) Temporary buildings and the storage of materials.
(9) Small scale bakeries, retail, including the manufacture of goods, and wholesales, including manufacture of goods.

Accessory Uses

The following shall be permitted on the Site as accessory uses and structures, subject to the applicable criteria set out in Chapter 12 of the Ordinance.

- (1) Accessory uses and structures clearly incidental and related to the permitted principal use or structure on the lot.
(2) Drive-in windows as an accessory to the principal use.
(3) Dumpsters, trash handling areas and service entrances.
(4) Fences and walls.
(5) Outdoor lighting.
(6) Outdoor storage of any materials, stocks or equipment.
(7) Vending machines for cigarettes, candy, soft drinks and coin-operated laundries located within an enclosed building as an accessory to the use in the principal building.

2. Notwithstanding anything contained herein to the contrary, the following uses are prohibited on that portion of the Site designated as Parcel 1 on the Technical Data Sheet: restaurants with drive-in window services, convenience stores (with or without gasoline sales), car washes, automobile service stations, gas stations, mental health institutions and alcohol and drug rehabilitation clinics. Except with respect to restaurants, accessory uses may include drive-in windows designed in accordance with the requirements of Section 12.413 of the Ordinance.

3. Notwithstanding anything contained herein to the contrary, the following uses are prohibited on that portion of the Site designated as Parcel 2 on the Technical Data Sheet: restaurants with drive-in window services, convenience stores (with or without gasoline sales), car washes, automobile service stations, gas stations, mental health institutions and alcohol and drug rehabilitation clinics. Additionally, notwithstanding anything contained herein to the contrary, drive-in windows shall not be permitted as an accessory use on Parcel 2. The buildings located along the southern boundary line of Parcel 2 adjacent to the 100 foot rear yard may not be devoted to any retail or restaurant use, and such buildings shall be limited in height to 1 story.

4. For purposes of this rezoning request and the development and use of the Site, any uses permitted by right in both the O-1 zoning district and the B-1 zoning district, other than general office, office/clinic (medical, dental, optical and veterinary) and financial institutions, shall be considered retail uses. Moreover, consumer oriented office uses such as fitness centers, weight loss centers, learning or educational centers and travel agencies shall be considered retail uses. General office uses, office/clinic uses and financial institutions shall be considered office uses.

5. The division of the Site into Parcels 1 and 2 on the Technical Data Sheet is solely for the purpose of this rezoning and it does not necessarily represent a future property line in the event that the Site is subsequently subdivided. The Petitioners do, however, reserve the right to pursue the subdivision of the Site in the future.

6. No piped or live music will be permitted outside of any buildings after 10:00 p.m.

7. Exterior courtyards or play yards that are accessory to any of the south boundary uses will be allowed if they are outside of the street setbacks or outside of the 85' class A buffer.

Maximum Floor Area

- 1. The gross heated floor area of all buildings constructed on the Site shall not exceed, in the aggregate, 95,000 square feet. Of this 95,000 square foot maximum gross heated floor area, a maximum of 55,000 square feet may be devoted to office uses and a maximum of 40,000 square feet may be devoted to retail uses. As provided in the Ordinance, the term "floor area" does not include any area used exclusively for the surface parking of motor vehicles or for building or equipment access, such as stairs, elevator shafts and maintenance crawl space.
2. Each building designated with a single asterisk on the Schematic Site Plan shall be limited in size to 5,000 square feet of gross heated floor area and shall be limited in height to 1 story.
3. Each building designated with two asterisks on the Schematic Site Plan shall be limited in height to 1 story.
4. Each building designated with three asterisks on the Schematic Site Plan shall be limited in height to 2 stories.
5. The maximum size of any single space occupied by a single office use shall be 10,000 square feet of gross heated floor area.
6. Outdoor porches and patios and outdoor seating and dining areas will not be considered to be a part of and counted towards the maximum gross heated floor area.

Setbacks, Side Yards and Rear Yards

1. All buildings constructed on the Site shall satisfy or exceed the setback and side yard requirements established under the Ordinance for the B-1 zoning district, and such setback and side yard requirements are more particularly depicted on the Technical Data Sheet. More specifically, a 40 foot setback shall be established along Tom Short Road, and a 40 foot setback shall be established along Ardrey Kell Road.

2. A 100 foot rear yard shall be established along the Site's southern boundary line as depicted on the Technical Data Sheet. Buildings and parking areas shall not be allowed within the 100 foot rear yard. The 85 foot Class A buffer described below under "Screening/Landscaping Areas/Buffer" shall be located within the 100 foot rear yard. Petitioners reserve the right to grade, install transformers and backflow, and underground utility lines, and to otherwise disturb that portion of the 100 foot rear yard not located within the 85 foot Class A buffer. Above ground utility lines may not be installed within that portion of the 100 foot rear yard not located within the 85 foot Class A buffer. Petitioners also reserve the right to locate the storm water detention facilities, or a portion thereof, within that portion of the 100 foot rear yard not located within the 85 foot Class A buffer.

Architectural Controls

1. Buildings constructed on the Site along Tom Short Road or Ardrey Kell Road will contain windows that face these streets and large expanses of solid walls for these buildings will be avoided through the introduction of architectural features such as storefront windows, access doors, articulated facades and other specially designed architectural elements. The windows on the first floor of any building facing Tom Short Road or Ardrey Kell Road shall not contain spandrel glass and shall be transparent. Second story windows in those buildings designated as Building H and Building I on the Schematic Site Plan that face Tom Short Road or Ardrey Kell Road are located within the cross-hatched areas depicted on the Schematic Site Plan shall contain spandrel glass and shall not be transparent.

2. The building elevations set out on Sheets RZ-3 and RZ-3.1 are intended to portray the basic character and quality of the relevant buildings to be constructed on the Site. Accordingly, these buildings shall be designed and constructed so that they are substantially similar in appearance to the building elevations set out on Sheets RZ-3 and RZ-3.1.

3. All buildings constructed on the Site shall be architecturally compatible through the use of architectural design features, similar materials, color and landscaping. At least 75% of the non-glass areas of building elevations shall be constructed with stone, brick, synthetic stucco or stucco materials with accents of hard-plank, synthetic stucco, etextco, pre-cast concrete or stone. Metal wall panels are not allowed.

4. The elevations of the buildings located along the southern boundary line of the Site adjacent to the 100 foot rear yard shall be as depicted on Sheet RZ-3.1.

5. Dumpster areas will be enclosed on all four sides, with three sides being a brick wall and one side being a hinged metal gate. If one or more sides of a dumpster area adjoin a side or rear wall of a building, then the side or rear wall may be substituted for a side. Beaded trash compaction (if any) will be screened with a gate on the access side.

6. Dumpsters shall not be located immediately adjacent to the 100 foot rear yard located along the Site's southern boundary line or immediately adjacent to Tom Short Road or Ardrey Kell Road.

7. All rooftop mechanical equipment shall be screened to its full height and shall not be visible from ground elevations along Tom Short Road or Ardrey Kell Road and ground elevations on adjacent residential uses. Rooftop mechanical equipment may not be installed on the buildings located along the southern boundary line of the Site adjacent to the 100 foot rear yard.

Streetscape Treatment

Subject to the approval of NCDOT and CDOT, Petitioners shall install an 8 foot wide planting strip and a six foot wide sidewalk along the Site's frontage on Ardrey Kell Road and Tom Short Road within the existing rights of way. To accommodate the sidewalks, a portion of each sidewalk may be located outside of the relevant right of way and the Petitioners will grant appropriate sidewalk easements in this event. Sidewalks may meander at the Petitioners' option to save existing trees.

Screening/Landscaping Areas/Buffer

- 1. Internal areas of the Site shall be landscaped in accordance with the requirements of the Ordinance.
2. The 85 foot Class A buffer established on the Technical Data Sheet shall conform to the standards for such buffers as set out in Section 12.502 of the Ordinance. This 85 foot Class A buffer shall remain as undisturbed open space except to the extent necessary to install, maintain and replace the wall described in paragraph 3 below, and except to the extent necessary to install, maintain and replace additional landscaping materials necessary to comply with the landscaping standards of a Class A buffer. The width of the Class A buffer has been reduced from 100 feet to 85 feet by virtue of the installation of the wall described in paragraph 3 below pursuant to Section 12.502(f) of the Ordinance. No further reductions in the width of the Class A buffer will be permitted.
3. Petitioners shall install an eight foot high masonry wall within the 85 foot Class A buffer a minimum of 75 feet from the Site's southern boundary line as more particularly depicted on the Technical Data Sheet.

4. All screening and landscaping shall conform to the standards of the Ordinance.

5. The Site shall comply with the requirements of the City of Charlotte Tree Ordinance.

6. Landscaped areas, hardscaped areas, sidewalks and site improvements will be installed in sequences which are keyed to each phase of development taking place on the Site. Notwithstanding the foregoing, the landscaped 40 foot setbacks along Tom Short Road and Ardrey Kell Road, and the 85 foot Class A buffer shall be installed at the time that the initial site improvements are constructed on the Site.

7. All trees planted within the Duke Power right-of-way are subject to the prior approval of Duke Power Company.

8. Petitioners shall install a berm and landscaping within the 40 foot setback along Tom Short Road and the 40 foot setback along Ardrey Kell Road in accordance with the detail and cross section set out on Sheet RZ-3. Notwithstanding the foregoing, Petitioners will not be required to install a berm in any location where the installation of the berm would result in the destruction of a tree protected under the terms of the Charlotte Tree Ordinance.

9. For those property owners who give their consent, Petitioners commit to plant a single row of Nellie Stevens hollies (or similar evergreens) at a minimum installation height of 4 feet and a maximum spacing of 8 feet on center. These plants will be planted along the top of the existing berm on the west side of Ardrey Kell Road directly across from the Site. Each property owner will be given notice of this planting option within 90 days after the requested rezoning is approved and will have a period of 60 days to accept or reject the offer. Planting will occur prior to the issuance of the first certificate of occupancy for a building constructed on the Site.

Off Street Parking

Off street parking will be provided at a ratio of 1 parking space per 250 square feet of space for each phase of development.

Lighting

- 1. All freestanding light fixtures installed within the Site shall be uniform in design. All parking lot lighting shall be metal halide.
2. The maximum height of any freestanding lighting fixture erected on the Site (other than streetlights on public rights-of-way), including its base, shall be 20 feet.
3. Any wall pack style lighting fixtures installed on the Site shall be capped and downwardly directed so that illumination does not extend past any property line of the Site.
4. All direct lighting within the Site (except streetlights which may be erected along Tom Short Road and Ardrey Kell Road) shall be designed such that direct illumination does not extend past any exterior property line. Consideration will be given to the impact of lighting both within and without the perimeter of the Site. Issues for consideration will include cutoff angles, color, energy efficiency and shielding of sources of light, the intent being to eliminate glare towards Tom Short Road and Ardrey Kell Road and adjacent properties.
5. Notwithstanding the terms of paragraph 4 above, the illumination from the decorative light fixtures to be installed on the building walls fronting Tom Short Road and Ardrey Kell Road and depicted on the attached elevations may extend past exterior property lines given the proximity of these buildings to Tom Short Road and Ardrey Kell Road. However, this illumination will not exceed .75 foot candles measured horizontally at grade at the exterior property lines. Illumination from such light fixtures, however, may not extend beyond the Site's southern property line.
6. The 20 foot light poles may not be installed within any interior landscaped islands within the parking areas, however, decorative lighting may be installed adjacent to sidewalks within any landscaped islands in the parking areas of the project.

Signs

- 1. All signage, including wall signage, installed on the Site shall conform to the requirements of Chapter 13 of the Ordinance.
2. Two project identification signs may be installed on the Site in accordance with the terms of the Ordinance, with a project identification sign being located at each of the two primary access points into the Site, and each project identification sign may include tenant identification signs. The project identification signs shall not be pole signs, shall not exceed 7 feet in height and shall be constructed with brick or stone and be compatible with the architectural design of the project. The maximum sign face area of each project identification signs shall be 50 square feet per side.
3. In addition to the project identification signs referred to in paragraph 2 above, that portion of the Site designated as Parcel 1 may have one tenant identification ground-mounted or monument type sign of no more than 4 feet in height with a maximum sign face area of 25 square feet per side. Notwithstanding the foregoing, this tenant identification sign may be incorporated into the 6 foot wall that may be installed at the northern tip of Parcel 1, provided that the sign copy may not exceed 25 square feet in size.
4. Any building mounted sign located immediately adjacent to and fronting either Tom Short Road or Ardrey Kell Road shall be indirectly lit with overhead or ground mounted lights rather than through internal illumination. However, all other signs may be lit indirectly or through internal illumination at the option of the Petitioners.

Access Points (Driveways)

- 1. The number of vehicular access points to the Site shall be limited to the number depicted on the Technical Data Sheet.
2. The placement and configuration of these access points are subject to any minor modifications required to accommodate final site and architectural construction plans and designs and to any adjustments required for approval by CDOT and/or NCDOT, if applicable.

Sidewalks

Sidewalks shall be provided as set forth above and as generally depicted on the Schematic Site Plan. Internal sidewalks shall have a minimum width of 5 feet.

Fire Protection

Fire protection in the form of fire hydrants will be provided on the Site, and a fire hydrant will be located within 750 feet of each building constructed on the Site. Plans for each building will be submitted to the Fire Marshal's office for approval before the construction of that building commences.

Storm Water Management

- 1. Storm water runoff from the Site will be managed through proven techniques which satisfy the standards imposed by the City of Charlotte.
2. Storm water detention shall not be located within any setback, except that the storm management flow restricting plate or structure may be located within the setback.
3. Storm water detention shall not be located within the 85 foot Class A buffer, however, storm water detention may be located within that portion of the 100 foot rear yard not located within the 85 foot Class A buffer.
4. The storm water detention facility will not be an amenity of the project.
5. Petitioners shall tie-in to the existing storm water system. Petitioners shall have the receiving storm drainage system analyzed to ensure that it will not be taken out of standard due to this development. If it is found that the development will cause the receiving storm drainage system to be taken out of standard, Petitioners shall provide adequate detention to prevent this from occurring. Should the existing receiving storm drainage system be deemed to be out of standard prior to the development of the Site, Petitioners agree to provide adequate detention to ensure that the system will not be additionally overburdened. The culvert should pass the 25 year storm event without overtopping the roads.

Transportation Commitments

- 1. As generally depicted on the Technical Data Sheet, the southern-most driveway accessing the Site from Ardrey Kell Road shall be located in such a way as to accommodate dedicated left turn lanes for both Landan Drive and the southern-most project entrance off of Ardrey Kell Road.
2. Subject to the approval of CDOT or NCDOT, whichever is applicable, Petitioners shall construct at their expense prior to the issuance of the first certificate of occupancy for a building constructed on the Site a left turn lane on southbound Ardrey Kell Road into the southern-most driveway accessing the Site from Ardrey Kell Road. The engineering, design and construction of this left turn lane will be the responsibility of Petitioners. It will be designed in accordance with the specifications of CDOT or NCDOT, whichever are applicable, and it will be designed in such a way as to accommodate dedicated left turn lanes for both Landan Drive and the southern-most project entrance off of Ardrey Kell Road. In the event that this left turn lane is not approved by NCDOT or CDOT, Petitioners will not be obligated to construct it.
3. Subject to the approval of CDOT or NCDOT, whichever is applicable, Petitioners shall construct at their expense prior to the issuance of the first certificate of occupancy for a building constructed on the Site a continuous common center left turn lane on Tom Short Road to provide left turn lanes into the Site's access driveways on Tom Short Road and a left turn lane into Warwick Links Drive. This continuous common center left turn lane shall extend from Ardrey Kell Road to that point which is located approximately 130 feet south of Warwick Links Drive. The engineering, design and construction of this continuous common center left turn lane will be the responsibility of Petitioners, and it will be designed in accordance with the specifications of CDOT or NCDOT, whichever are applicable. In the event that this continuous common center left turn lane is not approved by NCDOT or CDOT, Petitioners will not be obligated to construct it.
4. The northern-most driveway accessing the Site from Ardrey Kell Road shall be limited to right in movements only. To facilitate this limitation, Petitioners shall construct at their expense prior to the issuance of the first certificate of occupancy for a building constructed on the Site a median within Ardrey Kell Road in the location generally depicted on the Technical Data Sheet. In the event that the construction of this median is not approved by NCDOT or CDOT, Petitioners will not be obligated to construct it.
5. Subject to the approval of CDOT or NCDOT, whichever is applicable, Petitioners shall construct at their expense prior to the issuance of the first certificate of occupancy for a building constructed on the Site a left turn lane on northbound Ardrey Kell Road into Landan Drive. The engineering, design and construction of this left turn lane will be the responsibility of Petitioners, it will be designed in accordance with the specifications of CDOT or NCDOT, whichever are applicable, and it will be designed in such a way as to accommodate dedicated left turn lanes for both Landan Drive and the southern-most project entrance off of Ardrey Kell Road. In the event that this left turn lane is not approved by NCDOT or CDOT, Petitioners will not be obligated to construct it.
6. Bicycle racks shall be placed in various locations throughout the Site to accommodate one bicycle for every 20 off street parking spaces located on the Site.

6. Subject to the approval of CDOT or NCDOT, whichever is applicable, Petitioners shall construct at their expense prior to the issuance of the first certificate of occupancy for a building constructed on the Site a continuous common center left turn lane on Tom Short Road to provide left turn lanes into the Site's access driveways on Tom Short Road and a left turn lane into Warwick Links Drive. This continuous common center left turn lane shall extend from Ardrey Kell Road to that point which is located approximately 130 feet south of Warwick Links Drive. The engineering, design and construction of this continuous common center left turn lane will be the responsibility of Petitioners, and it will be designed in accordance with the specifications of CDOT or NCDOT, whichever are applicable. In the event that this continuous common center left turn lane is not approved by NCDOT or CDOT, Petitioners will not be obligated to construct it.

7. Subject to the approval of CDOT or NCDOT, whichever is applicable, Petitioners shall construct at their expense prior to the issuance of the first certificate of occupancy for a building constructed on the Site a left turn lane on northbound Ardrey Kell Road into Landan Drive. The engineering, design and construction of this left turn lane will be the responsibility of Petitioners, it will be designed in accordance with the specifications of CDOT or NCDOT, whichever are applicable, and it will be designed in such a way as to accommodate dedicated left turn lanes for both Landan Drive and the southern-most project entrance off of Ardrey Kell Road. In the event that this left turn lane is not approved by NCDOT or CDOT, Petitioners will not be obligated to construct it.

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10. Bicycle racks shall be placed in various locations throughout the Site to accommodate one bicycle for every 20 off street parking spaces located on the Site.

Amendments to Rezoning Plan

Future amendments to the Technical Data Sheet and these Development Standards may be applied for by the then Owner or Owners of the parcel or parcels within the Site involved in accordance with the provisions of Chapter 6 of the Ordinance.

Binding Effect of the Reasoning Documents and Definitions

- 1. If this Rezoning Petition is approved, all conditions applicable to development of the Site imposed under this Technical Data Sheet will, unless amended in the manner provided under the Ordinance, be binding upon and inure to the benefit of the Petitioners and the current and subsequent owners of the Site and their respective successors in interest and assigns.
2. Throughout these Development Standards, the term "Petitioners" shall be deemed to include the heirs, devisees, personal representatives, successors in interest and assigns of the Petitioners or the owner or owners of the Site from time to time who may be involved in any future development thereof.

CONDITIONAL REZONING PETITION NO. 2004-27

PETITIONERS:

BL PATCH & ASSOCIATES, INC.
2408-B PLANTATION CENTER DRIVE
MATTHEWS, NC 28108
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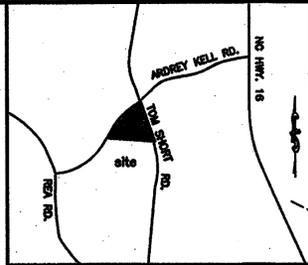
Scale:

The Fountains at Tom Short Road
BL Patch & Associates, Inc.
Charlotte, North Carolina

Table with project details: Project No., Date, Prepared By, Reviewed By, Date, Revision, and a table for Revision details (No., Description, Date).

Sheet Title: Technical Data Sheet

Sheet No: RZ-1.1



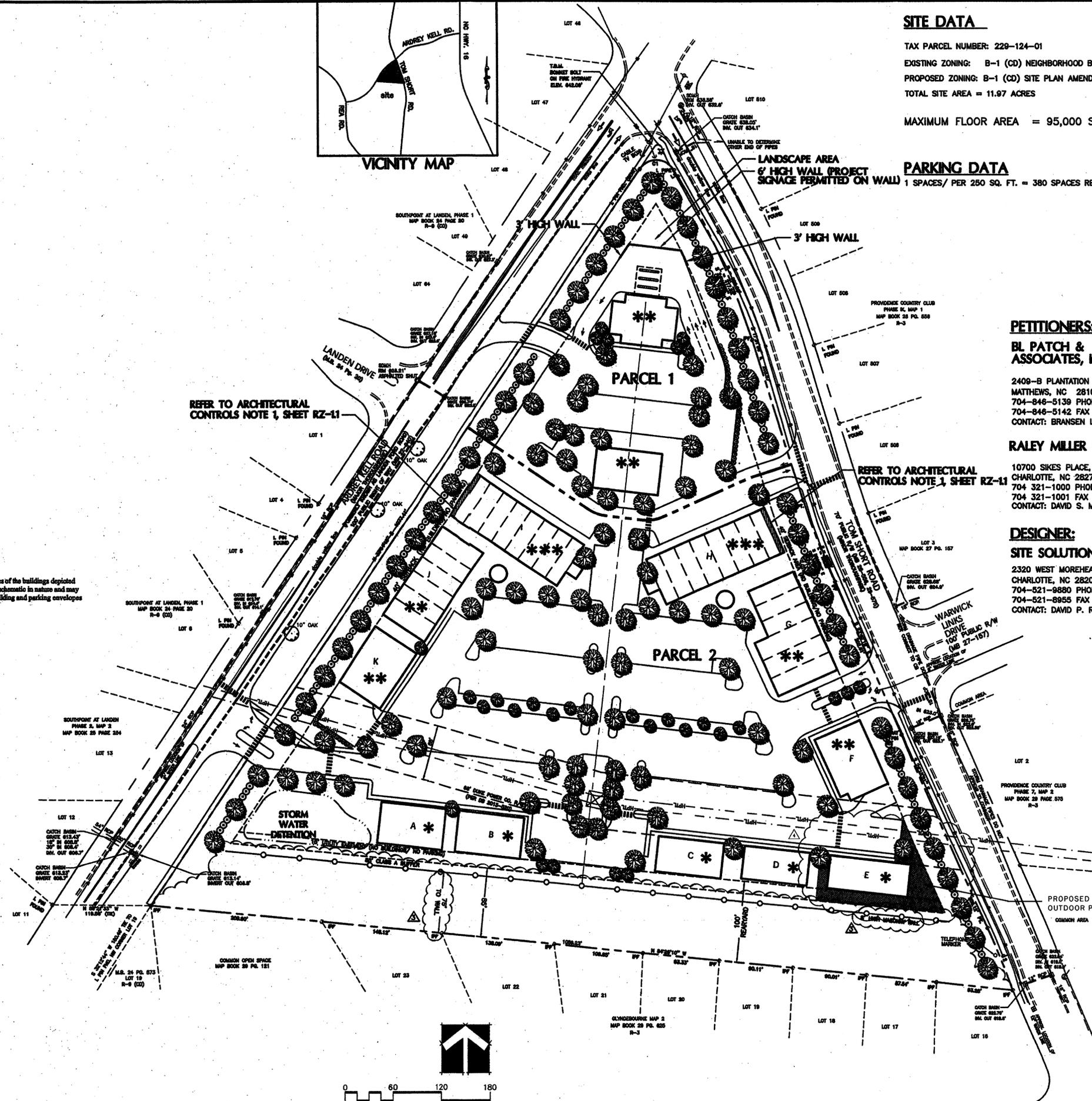
VICINITY MAP

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PARKING DATA

1 SPACES/ PER 250 SQ. FT. = 380 SPACES REQUIRED



REFER TO ARCHITECTURAL CONTROLS NOTE 1, SHEET RZ-11

REFER TO ARCHITECTURAL CONTROLS NOTE 1, SHEET RZ-11

Subject to the provisions of these Development Standards, the configurations, placements and sizes of the buildings depicted on the Schematic Site Plan, as well as the configurations, sizes and locations of parking areas, are schematic in nature and may be altered or modified during design development and construction document phases within the building and parking envelopes established on the Technical Data Sheet.

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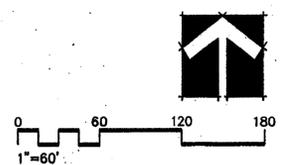
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 Charlotte, North Carolina

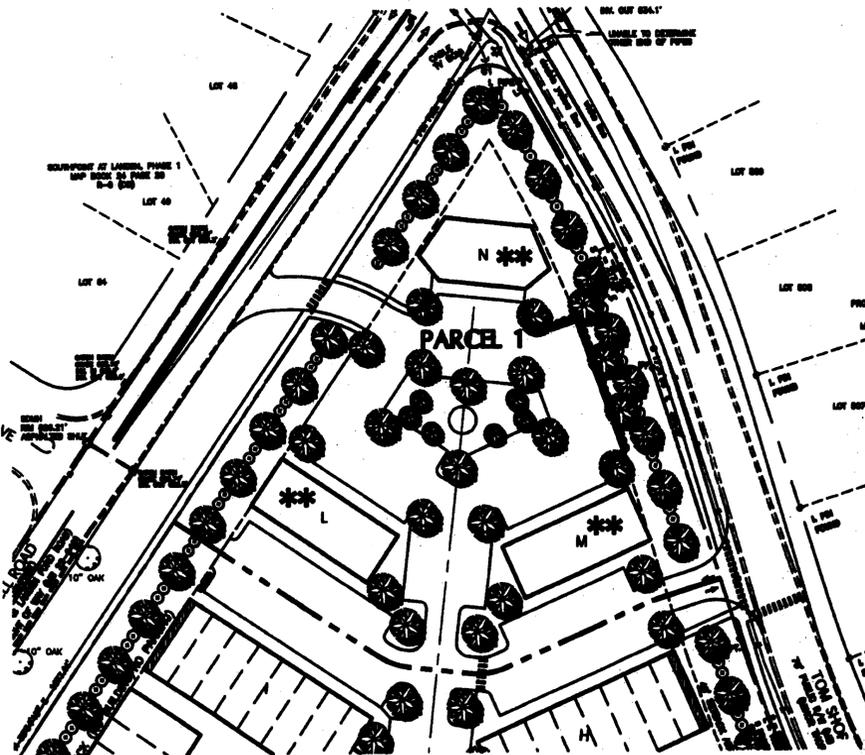
Project No:	289
Drawn By:	P. Wolf
Designed By:	D. Bear
Checked By:	D. Bear
Date:	11.15.04

Revisions:	
#12 Petition Submittal	4.15.04
#13 Rev. per CDDC	7.28.04
#14 Revision	1.15.04

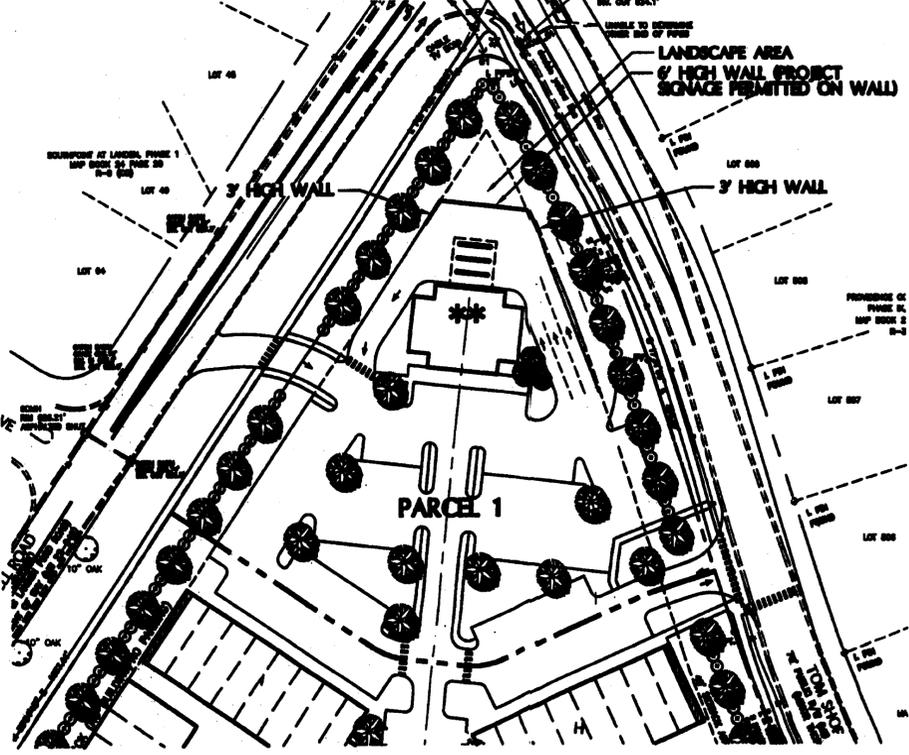
Sheet Title:
Schematic Site Plan

Sheet No:
RZ-2

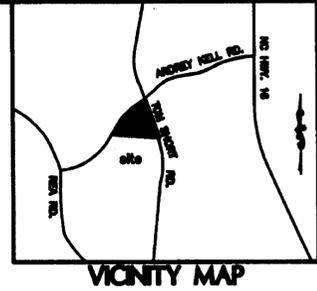




PARCEL 1 - DEVELOPMENT ALTERNATIVE (A-1)



PARCEL 1 - DEVELOPMENT ALTERNATIVE (A-2)



SITE DATA

TAX PARCEL NUMBER: 229-124-01
 EXISTING ZONING: B-1 (CD) NEIGHBORHOOD BUSINESS
 PROPOSED ZONING: B-1 (CD) SITE PLAN AMENDMENT
 TOTAL SITE AREA = 11.97 ACRES
 MAXIMUM FLOOR AREA = 95,000 S.F.

PETITIONERS:

BL PATCH & ASSOCIATES, INC.
 2409-B PLANTATION CENTER DRIVE
 MATTHEWS, NC 28105
 704-848-5136 PHONE
 704-848-5142 FAX
 CONTACT: BRANSEN L. PATCH

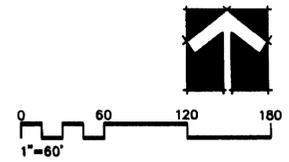
RALEY MILLER PROPERTIES
 10700 SIKES PLACE, STE. 200
 CHARLOTTE, NC 28277
 704-321-1000 PHONE
 704-321-1001 FAX
 CONTACT: DAVID S. MILLER

DESIGNER:

SITE SOLUTIONS, INC.
 2320 WEST MOREHEAD STREET
 CHARLOTTE, NC 28208
 704-521-9880 PHONE
 704-521-9855 FAX
 CONTACT: DAVID P. RECTOR, PE

SITE SOLUTIONS
 2 3 2 0
 W. Morehead Street
 Charlotte, NC 28208
 Landscape Architecture
 Site Planning
 Civil Engineering
 www.sitesolutionspa.com
 Telephone: 704-521-9880
 Facsimile: 704-521-9855

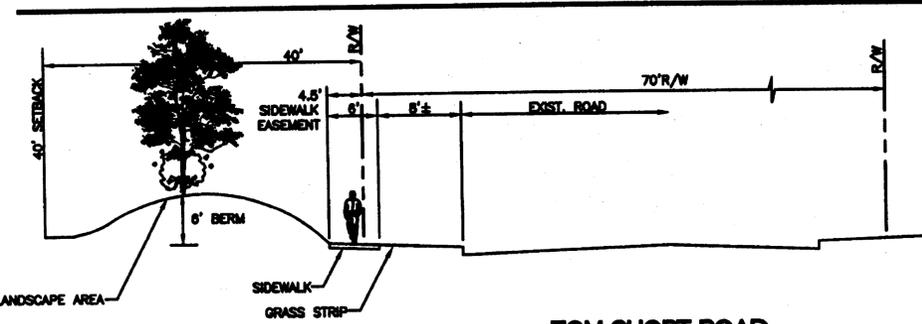
**The Fountains at
 Tom Short Road**
 BL Patch & Associates, Inc.
 Charlotte, North Carolina



Project No.	200
Drawn By	P.W.
Checked By	P.W.
Date	1/20/01
Revised:	
0/2 Pictorial Schedule	4/18/01

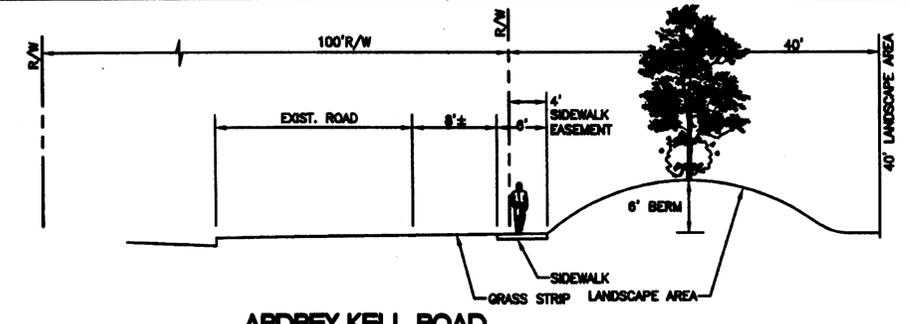
Alternative
 Schematic
 Site Plan

RZ-2.1



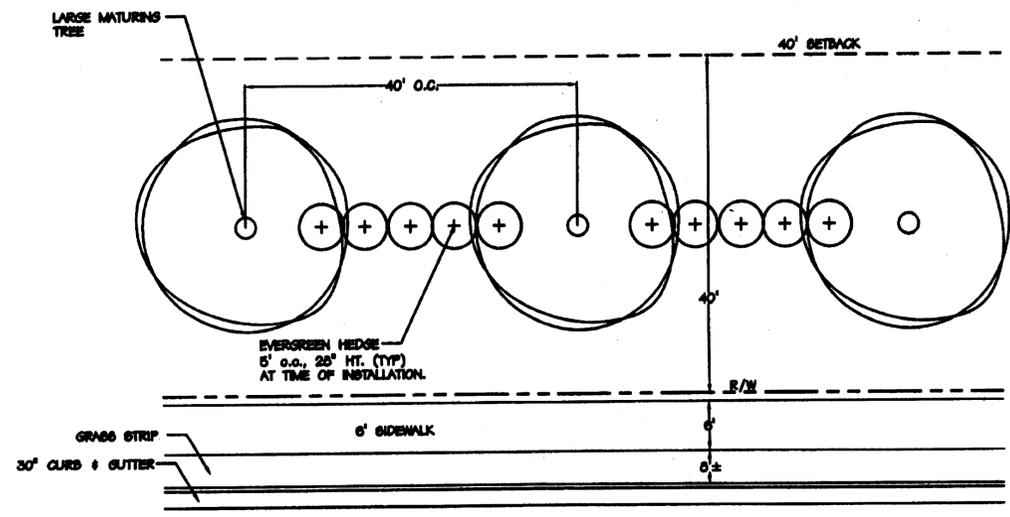
TOM SHORT ROAD

CROSS SECTION
 SCALE: 1"=10'
 NOTE: REFER TO "STREETSCAPE TREATMENT" UNDER THE DEVELOPMENT STANDARDS (TECHNICAL DATA SHEET, RZ-1) REGARDING SIDEWALK LOCATION.



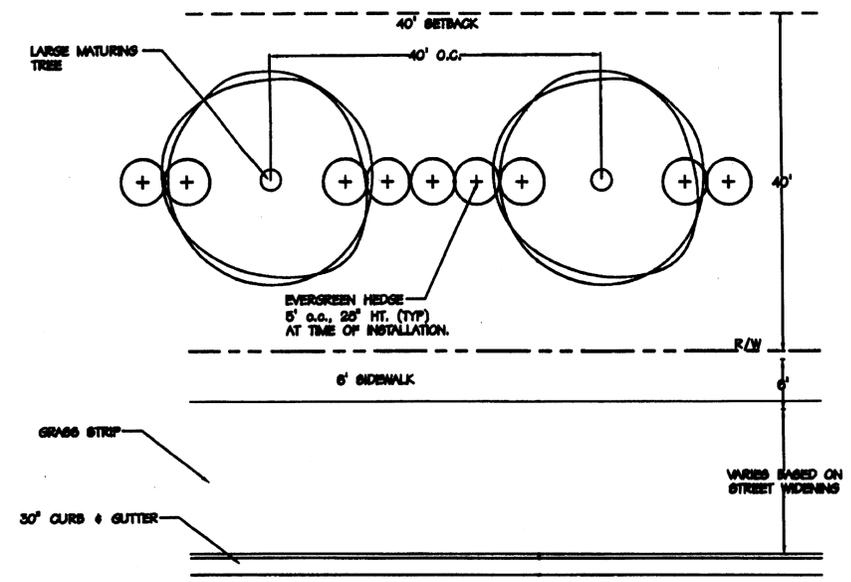
ARDREY KELL ROAD

CROSS SECTION
 SCALE: 1"=10'
 NOTE: REFER TO "STREETSCAPE TREATMENT" UNDER THE DEVELOPMENT STANDARDS (TECHNICAL DATA SHEET, RZ-1) REGARDING SIDEWALK LOCATION.



TYPICAL SETBACK PLANTING DETAIL TOM SHORT ROAD

SCALE: 1"=10'
 NOTE: REFER TO "STREETSCAPE TREATMENT" UNDER THE DEVELOPMENT STANDARDS (TECHNICAL DATA SHEET, RZ-1) REGARDING SIDEWALK LOCATION.



TYPICAL SETBACK PLANTING DETAIL ARDREY KELL ROAD

SCALE: 1"=10'



TYPICAL 1-STORY INTERIOR ELEVATION "ADJACENT TO THE 100' REAR YARD"
 NOT TO SCALE

The Fountains at Tom Short Road
 BL Patch & Associates, Inc.
 Charlotte, North Carolina

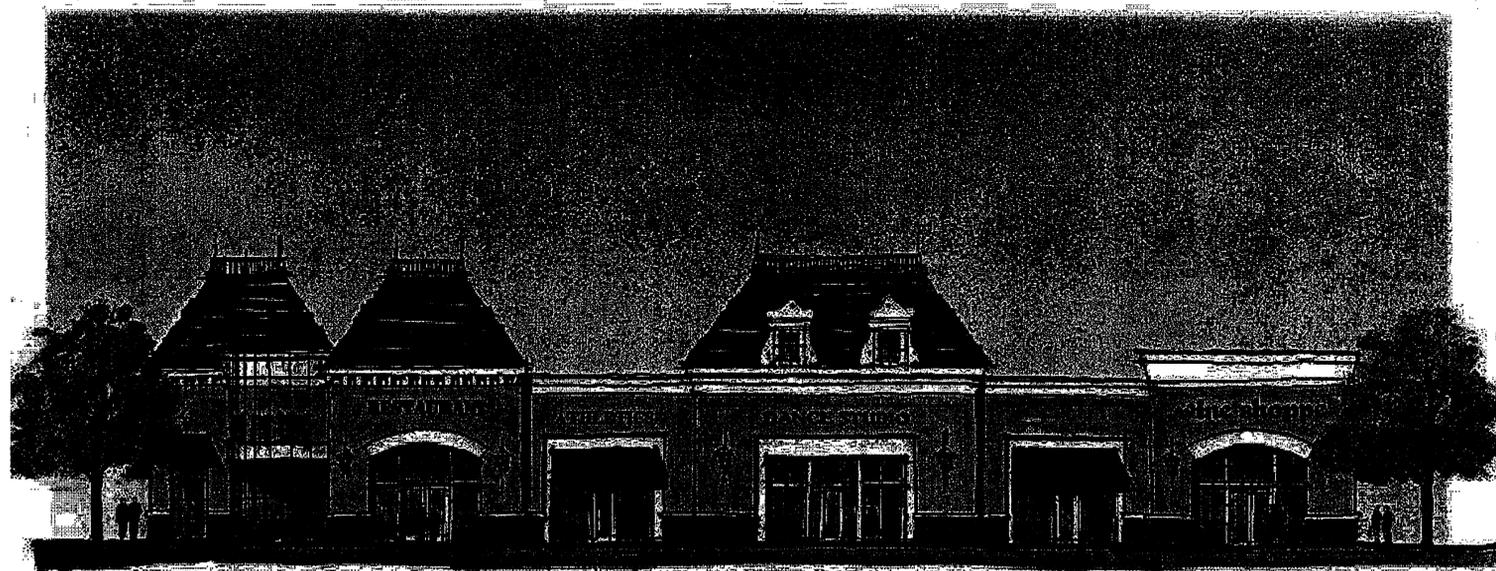
Project No:	200
Drawn By:	P. M.
Designed By:	B. B.
Checked By:	B. B.
Date:	4.18.04
Revised:	
#12 Permit Submittal	4.18.04

Sheet Title: **Building Elevations and Roadway Sections**

Sheet No: **RZ-3**



2- STORY OFFICE/RETAIL BUILDING ELEVATION
 LOOKING FROM THE INTERSECTION OF TOM SHORT & ARDREY KELL ROADS
 NOT TO SCALE



TYPICAL 1-STORY OFFICE/RETAIL BUILDING ELEVATION
 ALONG TOM SHORT & ARDREY KELL ROADS (STREET FRONT ELEVATION ONLY)
 NOT TO SCALE

The Fountains
 Shopping Mall Center

BL Patch & Associates
 Charlotte, North Carolina

SHOP NAMES ARE FOR ILLUSTRATION PURPOSES ONLY



SITE SOLUTIONS

2 3 2 0
 W. Morehead Street
 Charlotte, NC 28208

Landscape Architecture
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 Telephone-704-521-8880
 Facsimile-704-521-8855

Scale:

The Fountains at
 Tom Short Road
 BL Patch & Associates, Inc.
 Charlotte, North Carolina

Project No:	249
Drawn By:	P. Wat
Designed By:	D. Bester
Checked By:	D. Bester
Date:	4.16.04

Revisions:	
#2 Petition Submittal	4.16.04

Sheet Title: Building Elevations and Roadway Sections

Sheet No: RZ-3.1