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APPROVED BY CITY COUNCIL
DATE 5/16/05

PROVIDENCE PLAZA

FOR PUBLIC HEARING
PETITION NO. 2004-25

date: MARCH 28, 2005
status: REZONING RESUBMITTAL
revisions:

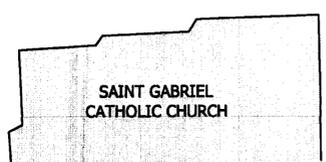
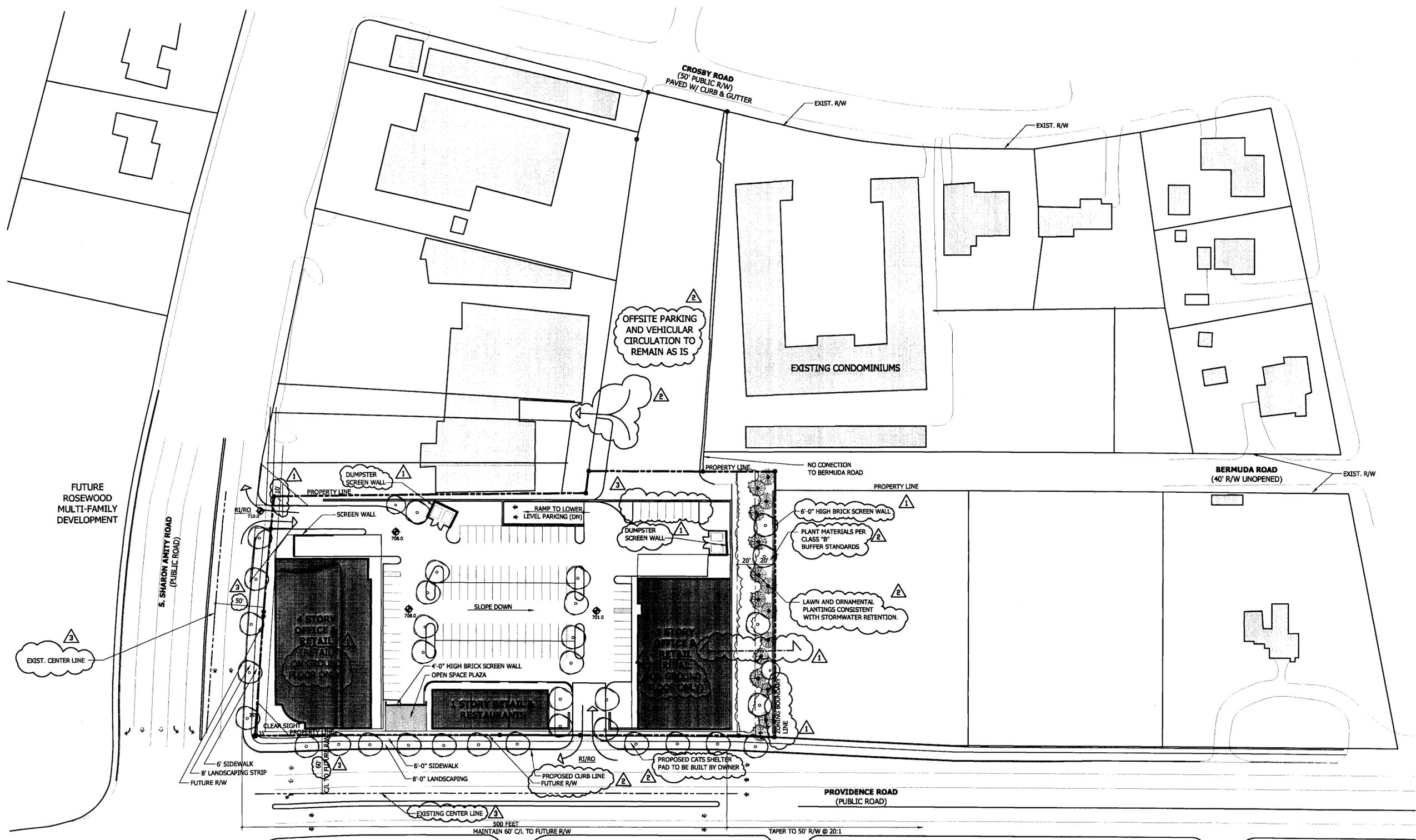
- △ 03.18.2005 per staff and community comments
- △ 03.28.2005 per additional staff comments
- △ 05.12.2005 per additional staff comments

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REZONING PLAN

RZ-1

ODA No. 041820 CADD File: 1820_RZ-1.dwg



SITE DATA:

PROPOSED ZONING: MUDD (CD) △

PROPOSED USE: OFFICE/ RETAIL/ RESTAURANT

ACERAGE: 3.14 ACRES

PROPOSED USES: △

RETAIL: 23,000 SF. (REQ. PARKING 1:250 = 92) △

RESTAURANTS: 12,000 SF. (REQ. PARKING 1:125 = 96)

OFFICE: 85,000 SF. (REQ. PARKING 1:300 = 283)

TOTAL: 120,000 SF. (TOTAL REQ. PARKING 471)

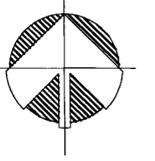
ON SITE PARKING: 114 ABOVE GRADE △
250 BELOW GRADE

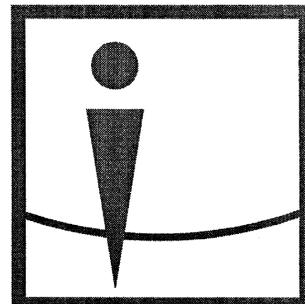
OFF SITE PARKING: 107 SPACES

TOTAL PARKING: 471 SPACES

LEGEND:

- 4" CALIPER - NEW TREES
- NEW PINE TREES





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**Providence Corner Site
MUDD Site Plan Notes**

Development of the site will be controlled by the standards depicted on this site plan and by the standards of the Charlotte Zoning Ordinance. The development depicted on this plan is intended to reflect the arrangement of proposed buildings and uses on the site, but the exact configuration, placement, and size of individual site elements may be altered or modified within the limits prescribed by the ordinance during the design development and construction phases.

Access to the site will be provided by driveway connections to Providence Rd., Sharon Amity Rd., as generally depicted on this site plan as well as to other properties through internal connections. The Petitioner will maintain the present connection through the site between Providence Rd. and Crosby Rd. These are the only access points and the site will not connect, either directly or indirectly, to Westbury Rd.

The proposed use of the property will be for the redevelopment of the site to accommodate a mixed-use development that may include retail, restaurant, and office uses along with associated parking and service areas. Building illustrations submitted as part of the Petition are intended to portray the style, character, typical features, mass, and scale of the buildings but are not intended to define specific design details that will be developed through the design development and technical review process.

All dumpsters will be screened with solid enclosures and gates.

The proposed development will comply with all applicable Ordinance requirements for signage, parking, screening, and landscaping except as provided for in number 6 below.

Parking will be provided which meets or exceeds the requirements of the Ordinance. Specifically, parking will be provided at a rate of one space for each 300 sq. ft. of office, for each 250 sq. ft. of retail, and for each 125 sq. ft. of restaurant floor area. This parking will be provided on site and off site to meet this requirement. Bicycle parking will be provided on the site in accordance with adopted bicycle parking standards. The Petitioner reserves the right to propose modifications to these standards if actual experience reveals that the standards result in a surplus or oversupply of parking. Such a proposal may be presented to the Planning Staff who may alter the parking requirements as an administrative amendment to the site plan. In addition, the Petitioner may make minor adjustment in the various floor area limits proposed on the site plan, by transferring floor area from one category to another, so long as the changes still comply with the parking requirements, the floor areas are not modified by more than 25% of the limits approved on the site plan, and the total floor area for the site is not increased.

Detached lighting on the site will be limited to 25' in height and the fixtures will be a full cutoff type. No 'wall pack' type lighting will be allowed but surface mounted architectural lighting will be permitted.

The Petitioner will provide for a CATS shelter outside of the right-of-way along the Providence Road frontage, the exact location of which will be determined jointly by CATS and the Petitioner.

The Petitioner will comply with adopted storm water standards. The Petitioner will tie into the existing storm water system(s). As part of the construction review process and if required by Stormwater Services, the Petitioner will analyze the downstream drainage system(s) to ensure that it will not be taken out of compliance by virtue of any action on the Petitioner's part. If it is determined that actions by the Petitioner will cause the downstream drainage system(s) to be "taken out of standard", the Petitioner will provide alternate methods to prevent this from occurring. Storm water detention, if required, will not be located between the buildings and the street or within designated buffer areas unless it is located underground or unless it is designed and maintained as part of the overall landscaping for the site.

The Petitioner acknowledges that other standard development requirements imposed by other city ordinances, such as those that regulate streets, sidewalks, and site development, may apply to the development of this site. These are not zoning regulations, are not administered by the Zoning Administrator, and are not separate zoning conditions imposed by this site plan. Unless specifically noted in the conditions for this site plan, these other standard development requirements will be applied to the development of this site as defined by those other city ordinances.

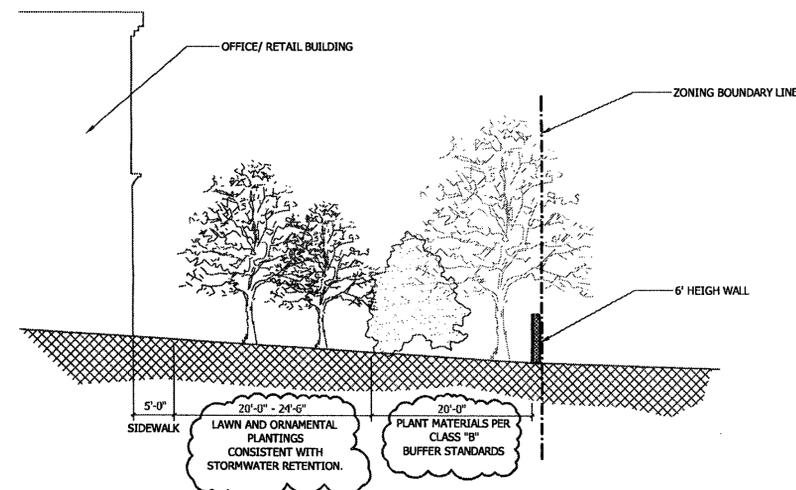
Throughout this Rezoning Petition, the terms "Owner", "Owners", "Petitioner" or "Petitioners," shall, with respect to the Site, be deemed to include the heirs, devisees, personal representatives, successors in interest and assignees of the owner or owners of the Site who may be involved in its development from time to time.

November 22, 2004, Initial submission.
Revised per staff and community comments, March 18, 2005
Revised per additional staff comments, March 24, 2005
Revised per Zoning Committee approval May 6, 2005



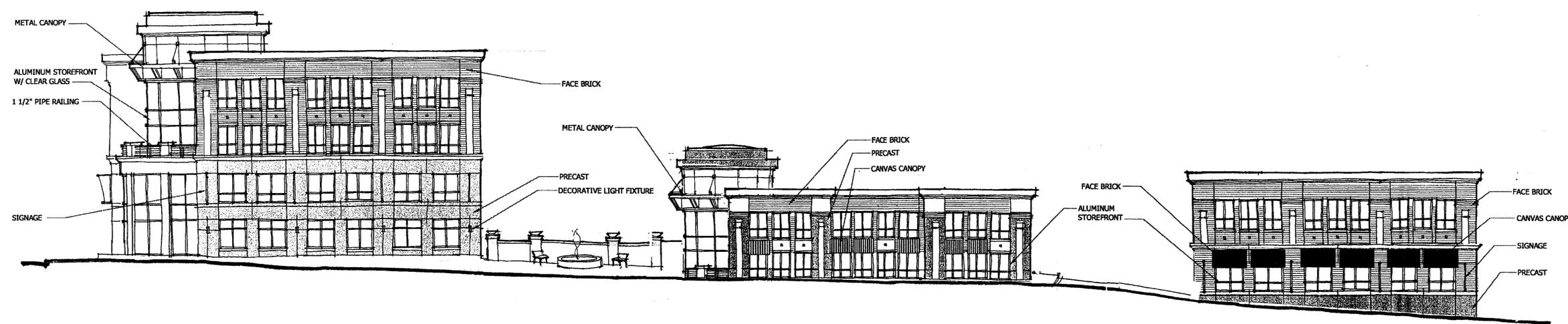
02 ELEVATION - S. SHARON AMITY ROAD

SCALE: 1'-0" = 1/16"



03 SECTION A-A

SCALE: 1'-0" = 3/32"



01 ELEVATION - PROVIDENCE ROAD

SCALE: 1'-0" = 1/16"

**PROVIDENCE
PLAZA**

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NOTES, ELEVATIONS
& SECTION

RZ-2