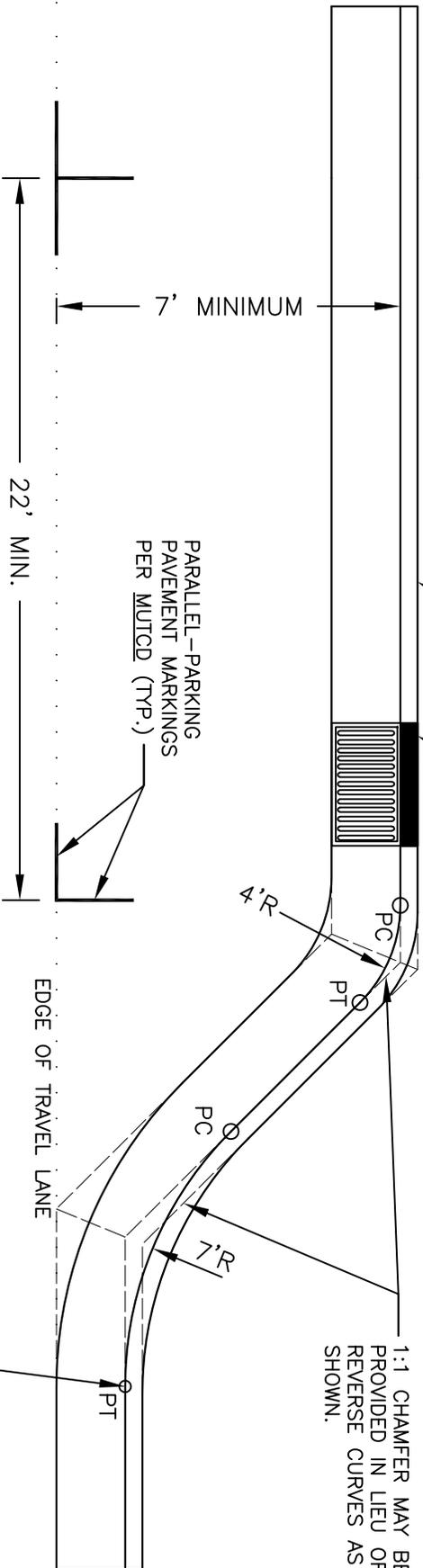


ADDITIONAL PARKING BAYS AND REVERSE CURVES/CHAMFERS AS APPROPRIATE.

2'-6" STANDARD CURB & GUTTER

PROVIDE CATCH BASIN OR SLOPE PARKING AREA TOWARD STREET FLOW LINE

1:1 CHAMFER MAY BE PROVIDED IN LIEU OF REVERSE CURVES AS SHOWN.



NOTES:

1. REVERSE CURVES/CHAMFERS NOT NECESSARY IF ADEQUATE DRAINAGE CAN BE PROVIDED THAT WILL ENSURE THAT SEDIMENT, WATER, DEBRIS, ETC., DOES NOT COLLECT IN 90-DEGREE CORNERS.
2. PARALLEL ACCESSIBLE SPACES AND LOADING ZONES TO BE REVIEWED BY CDOT ON A CASE-BY-CASE BASIS.
3. FOR PARKING BAYS THAT ARE 8 FEET IN WIDTH OR GREATER, THE PAVEMENT MARKINGS SHALL BE SET AT ONE (1) FOOT LESS THAN THE STALL WIDTH.
4. GREATER SEPARATION FROM INTERVENING STREETS THAN THE DISTANCES PROVIDED IN THE MATRIX MAY BE REQUIRED AT CDOT'S DISCRETION.
5. POSITIVE DRAINAGE SHALL BE PROVIDED EITHER BY INSTALLATION OF APPROPRIATE DRAINAGE STRUCTURES OR SLOPE PARKING AREA TO STREET FLOW LINE. SLOPING PARKING AREA TO STREET FLOW LINE ONLY PERMITTED IF ROAD GRADE IS GREATER THAN 2%.
6. IF A BIKE LANE IS REQUIRED ADJACENT TO PARALLEL PARKING, THE MINIMUM WIDTH OF BIKE LANE IS 6'.

MEASURE DISTANCE TO NEXT INTERVENING STREET OR ACCESSIBLE RAMP FROM THIS POINT. (SEE MATRIX BELOW)

PARALLEL PARKING BAY LOCATED ON

	DRIVEWAY	LOCAL/ COLLECTOR	TH'FARE
LOCAL/COLLECTOR	20'	20'	20'
THOROUGHFARE	20'	20'	50'

MINIMUM DISTANCE TO NEXT INTERVENING STREET

NOT TO SCALE



CITY OF CHARLOTTE
LAND DEVELOPMENT STANDARDS
 INCLUDES CHARLOTTE ETJ

PARALLEL PARKING STANDARDS

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