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WEEK IN REVIEW:

Mon (Sept 28)	Tues (Sept 29)	Wed (Sept 30)	Thurs (Oct 1)	Fri (Oct 2)
12:00 PM Governance & Accountability Committee, Room 280				
2:00 PM Budget Committee, Room 280				
5:00 PM Council Business Meeting, Room 267				
6:30 PM Citizens' Forum, Meeting Chamber				

CALENDAR DETAILS:

Monday, September 28

- 12:00 PM Governance & Accountability Committee, Room 280
AGENDA: Update on feedback of ethics policy from Chairs of boards and commissions; boards and commissions website; City Auditor's report; Enterprise risk management presentation; City Attorney's 2016 dimensions; City Manager 2016 dimensions
- 2:00 PM Budget Committee, Room 280
AGENDA: Compensation plan for non-exempt employees; Threshold for agenda item placement; Storm water ordinance; Follow-up from FY16 budget process & committee work plan
- 5:00 PM Council Business Meeting, Room 267
- 6:30 PM Citizens' Forum, Meeting Chamber

September and October calendars are attached.



Sept-Oct 2015.pdf

AGENDA NOTES:

Flu Shots Prior to Dinner Briefing, Room 267

Staff Resource: Alban Burney, CMO, 704 336-4947, aburney@charlottenc.gov

The Mecklenburg County Health Department will provide flu shots at the beginning of the Dinner Briefing in Room 267 for interested Council members. Staff has heard back from a couple of Council members who want to take advantage of this. If Council members haven't responded and want to receive a flu shot, please let staff know.

Citizens' Forum – Commercial Vehicles Violating City Code

Staff Resource: Jim Morrison, CMPD, jmorrison@cmpd.org, 704-432-3900

A resident of Brookwood Road has signed up to speak at Monday night's Citizens' Forum. This resident has complained to police about the heavy construction vehicle traffic along this residential street.

CMPD has observed a few heavy trucks in the area, and learned during traffic stops that GPS systems are routing the trucks onto Brookwood. The trucks do not see the signs prohibiting large trucks until they have turned on the road. CMPD has been working with CDOT and NCDOT to get signs posted on the main roads at either end of Brookwood that will warn drivers, before

they turn, that they are not permitted to use that street. In the meantime, CMPD will continue traffic enforcement in the area.

Citizens' Forum – Termination of Contract

*Staff Resources: Victoria Johnson, Solid Waste Services, 704-336-3410, vojohanson@charlottenc.gov
Thomas E. Powers III, City Attorney's Office, 704-336-5887, tpowers@charlottenc.gov*

K&S Sanitation is scheduled to speak at the Citizens' Forum on September 28, 2015, regarding the non-renewal of a private waste hauling services contract ("K&S Sanitation Contract") with the homeowners association for The Block At Church neighborhood.

The Block At Church is a townhome community consisting of fifty-nine multi-family units adjacent to Winnifred Street, W. Bland Street, South Church Street, and Lincoln Street. It is also contiguous with another parcel bordered by Winnifred Street, Lincoln Street, South Church Street, and Wilcox Street ("Phase II Property").

The original developer owned The Block At Church property and the Phase II Property. During the rezoning process for The Block At Church, the original developer designated a location for City-provided dumpster service on the Phase II Property for use by residents of The Block At Church. The original developer subsequently lost possession of the Phase II Property after the development of The Block At Church and those residents lost the ability to receive City-provided dumpster service.

Residents of The Block At Church informed the City of this, a belief that the City should provide service, and a desire to not renew its K&S Sanitation Contract. Concerns about space constraints and public safety led to the conclusion that Solid Waste Services could not provide City-provided dumpster service. Instead, Solid Waste Services offered to provide rollout container service.

The Block At Church agreed to the proposed solution and notified K&S Sanitation by letter dated July 31, 2015, that it would not be renewing the K&S Sanitation Contract. The K&S Sanitation Contract was scheduled to conclude on August 31, 2015.

K&S Sanitation subsequently contacted the City and requested monetary compensation from the City because The Block At Church chose not to renew the K&S Sanitation Contract. K&S Sanitation contends the City has violated (i) its policy by providing rollout container service to a multi-family property with thirty or more units (Policy), and (ii) North Carolina General Statutes § 160A-327 ("Displacement of private solid waste collection services").

Solid Waste Services made an exception to the Policy because of the unique circumstances regarding The Block At Church.

The City Attorney's Office believes North Carolina General Statutes § 160A-327 is not applicable to this matter because the K&S Sanitation Contract was scheduled to conclude on August 31,

2015. Starting on September 1, 2015, and thereafter, The Block At Church was free to choose any service provider for its waste hauling needs. Still, The Block At Church remained liable for any fees and costs associated with the K&S Sanitation Contract incurred prior to August 31, 2015.

Agenda Item #15 – Rezoning Petition 2014-109

Staff Resource: Tammie Keplinger, Planning, 704-336-5967, tkeplinger@charlottenc.gov

The updated attachments for this item, on Monday’s agenda, are attached below. Hard copies are included in Council’s packets.



2014-109_RevisedSitePlan_09-25-15.pdf

Agenda Item #32 – Interstate-85 North Bridge Planning and Owner’s Advisor

*Staff Resources: Jeb Blackwell, E&PM, 704-336-3603, jblackwell@charlottenc.gov
Danny Pleasant, CDOT, 704-336-3879, dpleasant@charlottenc.gov*

The September 28 Council agenda contains a contract for \$1.87 million in professional engineering services with HDR Engineering of the Carolinas. Under this contract, HDR will serve as the City’s Planning/Owner’s Advisor for the North Bridge project. Services will include project planning, community engagement, assisting staff in developing and documenting the design-build process necessary for future transportation projects, design- and construction-related support including technical review of structural and geotechnical submittals, and advising the City during the life of the design-build project.

This project, if approved, would be the City’s first transportation design-build project (Charlotte Water and City Storm Water Services have each undertaken design-build projects). The first phase of the project was funded with \$3.0 million in the 2014 bond. The remaining funding for the project will be provided by a combination of 2017 Federal Surface Transportation Program – direct allocation funds and proposed 2018 bond funding. Proposed funding from 2018 bonds was approved as part of the Council approved FY2015-FY2019 Community Investment Plan. The plan includes \$816.4 million to be placed on the ballot over four years- 2014, 2016, 2018 and 2020. The total project cost is estimated to be \$22.5 million. The projected completion timeframe with the current funding model is 2022.

The schedule for the project completion can be accelerated if bond funding was made available in the 2016 bond referendum. (Schedule comparison is provided in Attachment A). Consideration of the merits of accelerating the funding for this project should be included in future discussion of the overall 2016 CIP adjustments. Approving the Planning/Owner’s Advisor contract at the September 28 Council meeting does not commit the City to accelerating the funding, but does preserve that possibility for consideration next year. Since the first 30% of the design is the same in either delivery method there is no risk to proceeding with this option at this time.

Additional information about Design-Build, the I-85 North Bridge Project, schedule implications and the Engineering Professional Services for the I-85 North Bridge Project are included in [Attachment A](#).



Attachment A.pdf

Agenda Item #47 – LYNX Blue Line Extension Sugar Creek Parking Garage

Staff Resources: John Lewis, CATS, 704-336-3855, john.lewis@charlottenc.gov

Danny Rogers, CATS, 704-432-3033, drogers@charlottenc.gov

This attached RCA is part of the September 28 City Council Agenda.

Item #47 will ask City Council to award a construction contract for the Blue Line Extension Sugar Creek Parking Garage. Staff could not finalize the item in time for the printing of the standard City Council Agenda because a Disadvantaged Business Enterprise (DBE) procedure, required by federal law, was still on-going at that time. That process has now been completed.

Staff will be recommending that the contract be awarded to Messer Construction in the amount of \$23,346,850. Messer was the lowest bidder that met all of the requirements to be considered responsive. The lowest bid received was from Archer-Western Construction LLC, in the amount of \$22,726,999.63. However, staff found that Archer-Western's bid was non-responsive as it did not meet the DBE goal and did not evidence adequate 'Good Faith Efforts'. This finding was upheld after an Administrative Reconsideration Hearing was held.

Representatives of both Archer-Western and Messer Construction may appear at the City Council meeting and wish to speak regarding this Item.



Agenda Item
#47.pdf

Agenda Item #55 – Airport Escalator and Elevator Maintenance Services Contract Amendment

Staff Resource: Jack Christine, Aviation, 704-359-4932, jchristine@cltairport.com

At the September 28 Council Business meeting, the Aviation Department will request that the City Council approve Amendment #5 to the Airport's maintenance contract with Schindler Elevator Corporation. This amendment will provide for the complete refurbishment of two moving walkways in the Terminal Atrium. This Council-Manager Memo provides historical context for the Schindler contract through the present and forecasts future funding requests for equipment refurbishment or replacement and the addition of new equipment in the coming years.

On June 10, 2013, the City Council approved a five-year maintenance contract with Schindler for \$561,480 annually to cover the routine maintenance of the Airport's elevators, escalators, and moving walks. This contract provides that Schindler will address non-routine maintenance, (e.g. equipment refurbishment or replacement, or the extension of the maintenance services to newly added equipment) through separate price quotes incorporated by contract amendment. The Aviation Director approved Amendment #1, which provided for several non-monetary modifications to the contract. The City Manager subsequently approved Amendment #2 for the purchase of spare parts. Amendment #3, approved by the City Council in May of 2014, provided for the refurbishment of two escalators that had exceeded their useful lives.

In November 2014, the Airport hired a consultant to assess the condition of its elevators, escalators and moving walks. The consultant concluded that multiple units of equipment were either at or near the end of their useful lives necessitating refurbishment or replacement. Amendment #4, approved by the City Council in January 2015, began the modernization program mapped out by the consultant by refurbishing two moving walks, two escalators, and three elevators. In addition, Amendment #4 extended the routine maintenance services to 17 new escalators and elevators installed in the Rental Car and Hourly Parking Deck that was opened in late 2014.

Amendment #5 continues the modernization process the Airport began with Schindler nearly three years ago, which will enable the Airport to refurbish two moving walks in the Terminal Atrium that have been in service since 1992. By using Schindler for this work, the Airport can realize cost savings by crediting Schindler's fees for routine scheduled maintenance against the planned refurbishment.

The Airport anticipates that additional work will be necessary to ensure that all Airport elevators, escalators, and moving walks are operating at peak capacity for our passengers' convenience. The Airport is under contract with Schindler through June 30, 2018 with two one-year options that would complete the contract in 2020. The Airport plans to continue using Schindler's services for equipment modernization if and when such projects become necessary. Any such projects will be brought to the City Council for its review and consideration at the appropriate time. At the conclusion of this contract, the Airport will re-solicit for these maintenance services.

Agenda Item #56 – Airport Concourse A North Expansion Phase I Design Services

Staff Resource: Jack Christine, Aviation, 704-359-4932, jlchristine@cltairport.com

At the September 28 Council Business Meeting, City Council will be asked to approve a design contract with Perkins+Will North Carolina for the Concourse A North Expansion Project, in the amount of \$7,825,982.00. On December 12, 2014, the Aviation Department issued a Request for Qualifications for architectural and engineering design services for the Concourse A North Expansion; eight firms responded with a proposal. Perkins+Will North Carolina was selected based on their related concourse design experience.

This will be the first of several contracts that City Council will be asked to approve for the Expansion of Concourse A. The Aviation Department intends to bring forward a Construction Manager at Risk Contract for the construction of the project for approval at the December 14 City Council meeting.

The Concourse A North Project will construct a new concourse pier that will encompass 211,000 square feet and nine additional gates, a connector to the existing A Concourse, concession space, and office space. The new building will be designed to achieve LEED Certification. This project will be the first phase of new concourse construction north of existing A Concourse. The total build out will include a total of 24 new gates. This first phase of construction is scheduled to be completed in March 2018.

INFORMATION:

Council Agenda Deliveries

Staff Resources: Katie McCoy, M&FS, 704-336-5017, kdmccoy@charlottenc.gov

Since transitioning to the new automated agenda in June, the Wednesday Council packets with the Business and Zoning Agendas have been delivered much later in the evening than usual. This is due to the difficulty producing hard copy agendas. Staff is continuing to explore manual processes to create and print these agendas. To avoid late evening deliveries, staff will make a special delivery of those hard copy agendas on Thursday, if documents are not ready by delivery time on Wednesday.

The agendas will continue to be sent electronically on Wednesday as well as be available on the iLegislate application and posted online at:

<http://charmeck.org/city/charlotte/CityClerk/Pages/CouncilMeetingsandDocuments.aspx>.

Should you have any questions about this issue or desire more training on the electronic version, please contact Jennifer Holland (jholland@charlottenc.gov ; 704-336-2449) in Management & Finance Services or Alban Burney (aburney@charlottenc.gov; 704-336-4947) in the City Manager's Office.

Blue Line Extension Financing

Staff Resources: Randy Harrington, Management & Financial Services, 704-336-5013,

rijharrington@charlottenc.gov

John Lewis, CATS, 704-336-3855, john.lewis@charlottenc.gov

On September 17, 2015 the City closed the final phase of financing for the CATS Blue Line Extension (BLE). The \$180 million long-term loan was granted under the Transportation Infrastructure Finance and Innovation Act (TIFIA) from the US Department of Transportation. This loan will be repaid over 33 years at a rate of 3.07%, which is below what the City could

have obtained from private markets. The TIFIA loan reduces borrowing cost by approximately \$32 million over the life of the loan compared to traditional financing.

The \$1.16 billion Blue Line extension will add 9.3 miles to the existing LYNX system and will include 11 stations, 3,000 parking spaces, and carry 24,500 daily passengers by 2035. The sources of funding for the BLE are as follows:

Source	Amount
Federal Grant	\$580 million
State Grant	\$299 million
Local Funding (including \$180M TIFIA loan)	\$281 million
Total	\$1,160 million

The Transit Funding Working Group, a subcommittee of the Metropolitan Transit Commission, originally recommended this approach to financing the local portion of the BLE. CATS and the Management & Financial Services department have worked on securing TIFIA funding for the past two years.

Characteristics of Charlotte Housing Authority’s Housing Choice Voucher Report

*Staff Resource: Pamela Wideman, NBS, 704.336-3488, pwideman@charlottenc.gov,
Mary Gaertner, NBS, 704-432-5495, mgaertner@charlottenc.gov*

On Monday, September 28, UNC Charlotte Urban Institute, in partnership with the Charlotte Housing Authority (CHA) and Housing Advisory Board of Charlotte-Mecklenburg, will release a report on the characteristics of CHA’s Housing Choice Voucher Program wait list. The report is one in a series of reports sponsored by Mecklenburg County on housing instability and homelessness. The series includes the recently published, Point-In-Time Count Report, which provides the number of individuals and families who are homeless on a given night in Charlotte. Forthcoming reports include Veteran’s Homelessness, which will describe the population of veterans who have experienced homelessness, and annual counts of homelessness in Mecklenburg County.

The Housing Choice Voucher Program is formerly known as the Section 8 Program, and is a federally funded rental assistance program that subsidizes rents for low-income households renting in the private market. In September 2014, the waiting list was opened for the first time since 2007, and resulted in approx. 30,000 individuals applying for housing assistance.

The findings from this report highlight the overwhelming need for affordable housing and the disproportionately high amount of African-Americans in need of housing assistance. Research supports the effectiveness of housing vouchers in reducing housing instability, homelessness, and overcrowding, and the report will be used to further community conversations on resource allocation and economic mobility.

Mayor and City Council members will receive a copy of the report via email on Monday, September 28, 2015.

FY2015 Year-End City of Charlotte Performance Report

Staff Resource: Rachel Wood, M&FS, 704-336-3656, rwood@charlottenc.gov

The City's Fiscal Year 2015 Year-End Performance Report is attached and a hard copy is provided in today's Council packet. The Year End Report is organized according to City Council's five Focus Areas, highlighting key achievements and the results of notable performance measures from across the organization.

The electronic version includes hyperlinks that provide additional information. These hyperlinks are denoted in underlined text. The FY2015 Year-End Report is available online at: <http://charmeck.org/city/charlotte/MFS/budget/Pages/planning.aspx>.

Additional hard copies are available by contacting Rachel Wood.



FY15 Performance
Report.pdf

Mayor's Youth Employment Program Begins with Career Readiness Training Certification

Staff Resource: Steve Wood, NBS, 704-336-4161, swood@charlottenc.gov

The Mayor and City Council Members are invited to attend graduation ceremonies for the Mayor's Youth Employment Program (MYEP) Career Readiness Training Certification, the first component of the 2016 MYEP season. Training sessions will be held September 30 – November 18, 2015, during the school day at sixteen campuses throughout CMS. The graduation ceremony will immediately follow. The attached schedule provides locations, dates, and times of the ceremonies.

Approximately 700 students are expected to complete the MYEP Career Readiness Training Certification. Participants will receive training in job readiness, customer service, and financial literacy, making them better prepared for success in their future employment opportunities and careers. Following the training, students will be able to pursue a variety of career exploration pathways, including MYEP summer internships, job shadowing opportunities, and direct employment.

For more information on MYEP's Career Readiness Training Certification or to RSVP to attend one of the graduation events, please contact Steve Wood at swood@charlottenc.gov.



092515_CRTC
Schedule.pdf

Community Investment Plan Newsletter

Staff Resources: Ken Brown, Corporate Communications & Marketing, 704-336-5863, kebrown@charlottenc.gov

Attached is the September edition of the Community Investment Plan (CIP) newsletter. This newsletter highlights the efforts around the CIP projects, provides status reports and notifies Council of any upcoming meetings with residents and stakeholders. If Council members are aware of additional engagement opportunities, please contact Ken Brown.



September 2015 CIP
Newsletter.pdf

2015 State Legislative Report #32

Staff Resource: Dana Fenton, City Manager's Office, 704-336-2009, dfenton@charlottenc.gov

Attached is the latest State Legislative Report.



2015 week 32
report.pdf

September

<i>Sun</i>	<i>Mon</i>	<i>Tue</i>	<i>Wed</i>	<i>Thu</i>	<i>Fri</i>	<i>Sat</i>
		1	2	3	4	5
6	7 Labor Day	8	9 12:00pm Housing & Neighborhood Dev. Committee Mtg., Room 280 2:00pm Environment Committee Mtg., Room 280	10 12:00pm Community Safety Committee Mtg., Room 280	11	12
13	14 3:00pm Transportation & Planning Committee Mtg., Room 280	15	16	17 12:00pm ED & Global Competitiveness Committee Mtg., Ovens Auditorium, Hospitality Room, 2 nd Floor	18	19
20	21 3:00pm Intergovernmental Relations Committee Mtg., Room 280 5:00pm Zoning Meeting, Room CH-14	22	23 12:00pm Housing & Neighborhood Dev. Committee Mtg., Room 280 5:30pm MTC Meeting, Room 267	24 12:00pm Transportation & Planning Committee Mtg., Room 280	25	26
27	28 12:00pm Governance & Accountability Committee Mtg., Room 280 2:00pm Budget Committee Mtg., Room 280 5:00pm Citizens' Forum/Council Business Meeting, Room 267	29	30			
ICMA Conference Seattle, WA				2015		

October

<i>Sun</i>	<i>Mon</i>	<i>Tue</i>	<i>Wed</i>	<i>Thu</i>	<i>Fri</i>	<i>Sat</i>	
				1	2	3	
4	5 5:00pm Council Workshop/Citizens' Forum, Room 267	6	7	8 12:00pm Community Safety Committee Mtg., Room 280	9	10	
11	<div style="border: 1px solid black; background-color: #cccccc; padding: 5px; text-align: center;"> NCLM Conference Winston-Salem, NC </div>		14 2:00pm Environment Committee Mtg., Room 280	15 12:00pm ED & Global Competitiveness Committee Mtg., Room CH-14	16	17	
			18	19 5:00pm Zoning Meeting, Room CH-14	20	21 2:00pm Housing & Neighborhood Dev. Committee Mtg., Room 280	22 12:00pm Transportation & Planning Committee Mtg., Room 280
25	26 12:00pm Governance & Accountability Committee Mtg., Room 280 2:00pm Budget Committee Mtg., Room CH-14 5:00pm Citizens' Forum/Council Business Meeting, Room 267	27	28 5:30pm MTC Meeting, Room 267	29	30	31	

2015

This site plan is for the purpose of rezoning only and does not reflect a current boundary or physical survey of the property shown. Boundary lines and physical improvements were taken from prior surveys by this firm. This map is not a certified boundary or physical survey and does not meet G.S. 47-30 as amended.

PRELIMINARY - FOR REVIEW PURPOSES ONLY.

Andrew G. Zoutewelle, N.C. PLS L-3098

GENERAL NOTES

- 1.) Source of title recorded in: Deed Book 26168 Page 362 (tax parcel I.D. 12522801), Deed Book 20671 Page 784 (tax parcel I.D. 12522102 and 12522199), and Deed Book 24096 Page 836 (tax parcel I.D. 12522103). See also Deed Book 28261 Page 510.
- 2.) Total area of property is 1.9922 acres (or 86,780 S.F.), by coordinates. This area includes portions of Cecil Street which is to be abandoned.
- 3.) This survey was done without the benefit of a title examination. There may be easements, restrictions or other matters of title not shown.
- 4.) Existing zoning of these properties, per Mecklenburg County GIS, is UR-C(CD), R-8 and B-1. Zoning boundary lines shown hereon are taken from Mecklenburg County GIS and the City of Charlotte Official Zoning Map. Development of this property and setback configuration is subject to the review and approval of the City of Charlotte Zoning Staff. This survey does not reflect a complete zoning analysis.
- 5.) This site plan does not show any utilities which exist on this site. It is the contractor's responsibility to verify the existence and location of all utilities before any grading or excavation is begun.
- 6.) Per the City of Charlotte Thoroughfare Map, South Kings Drive is a Major Thoroughfare and may be subject to an 80' proposed thoroughfare road right of way (40' from centerline). The City of Charlotte DOT should be consulted to determine the applicability of said right of way prior to any design or development along S. Kings Drive.
- 7.) This property is not located within a designated flood hazard area per graphic scaling from Flood Insurance Rate Map Panel #3710455300K dated February 19, 2014.
- 8.) Grid ties provided by Survey and Mapping Control, Pineville, N.C. All distances shown hereon are horizontal ground distances. Combined Grid Factor = 0.99984618. Horizontal datum is NAD83(CORS96).

LEGEND

ch.	chord
L,R	curve length & radius s.f.
s.f.	square feet (by coordinates)
IPF/IRF	iron pipe/rebar found
IRS	iron rebar (pin) set
PKS	P.K. nail set
PKF	P.K. nail found
MB,DB	record map, deed reference
⊕	guy anchor
⊖	power pole
⊙	light pole
⊗	water meter
⊘	water vault
⊙	gas meter
⊙	fire hydrant
⊙	telephone manhole
⊙	sanitary sewer manhole
⊙	sanitary sewer clean-out
⊙	storm sewer manhole
⊙	catch basin
⊙	power meter
*FDC	Fire Dep't. Connection
⊕	water valve
TSP	traffic signal pole
TSV	traffic signal vault
PDE	Permanent Drainage Easement
ZBL	zoning boundary line (approx.)

Tax I.D. 12521413
Midtown Area Partners LLC
Zoning: B-1

CURVE TABLE

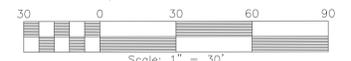
NUMBER	RADIUS	ARC LENGTH	CHORD DIRECTION	CHORD LENGTH
C1	25.50	40.36	N 04°13'37" E	36.28
C2	524.50	34.40	N 47°41'53" E	34.39
C3	22.30	38.00	S 33°52'53" E	33.57
C4	21.64	35.74	S 62°57'24" W	31.81
C5	1279.70	15.40	N 71°37'50" W	15.40
C6	208.18	35.83	N 46°03'10" W	35.79
C7	208.18	44.24	N 57°04'19" W	44.16
C8	524.50	37.91	N 43°44'55" E	37.91
C9	25.00	32.30	N 78°41'10" E	30.10

LINE TABLE

NUMBER	DIRECTION	DISTANCE
L1	S 64°18'20" E	50.17'
L2	S 61°26'21" E	49.74'
L3	S 60°28'58" E	24.57'
L4	S 60°43'01" E	48.59'
L5	S 58°27'53" E	2.88'
L6	S 58°27'53" E	36.89'
L7	N 64°28'27" W	17.78'
L8	S 16°13'19" W	11.74'
L9	S 16°13'19" W	233.81'
L10	N 62°53'25" W	14.96'
L11	S 64°18'20" E	2.26'
L12	S 64°18'20" E	17.74'
L13	S 16°13'19" W	175.43'

Revised 10-28-14: Show Cecil Street abandonment area.
Revised 03-19-15: Add development areas.
Revised 05-20-15: Add calls for development areas. Copyright 2015

SITE PLAN
**South Kings Drive at
Cecil and Luther Streets**
CHARLOTTE, MECKLENBURG COUNTY, N.C.
for MIDTOWN AREA PARTNERS II, LLC
Date of Map: October 15, 2014



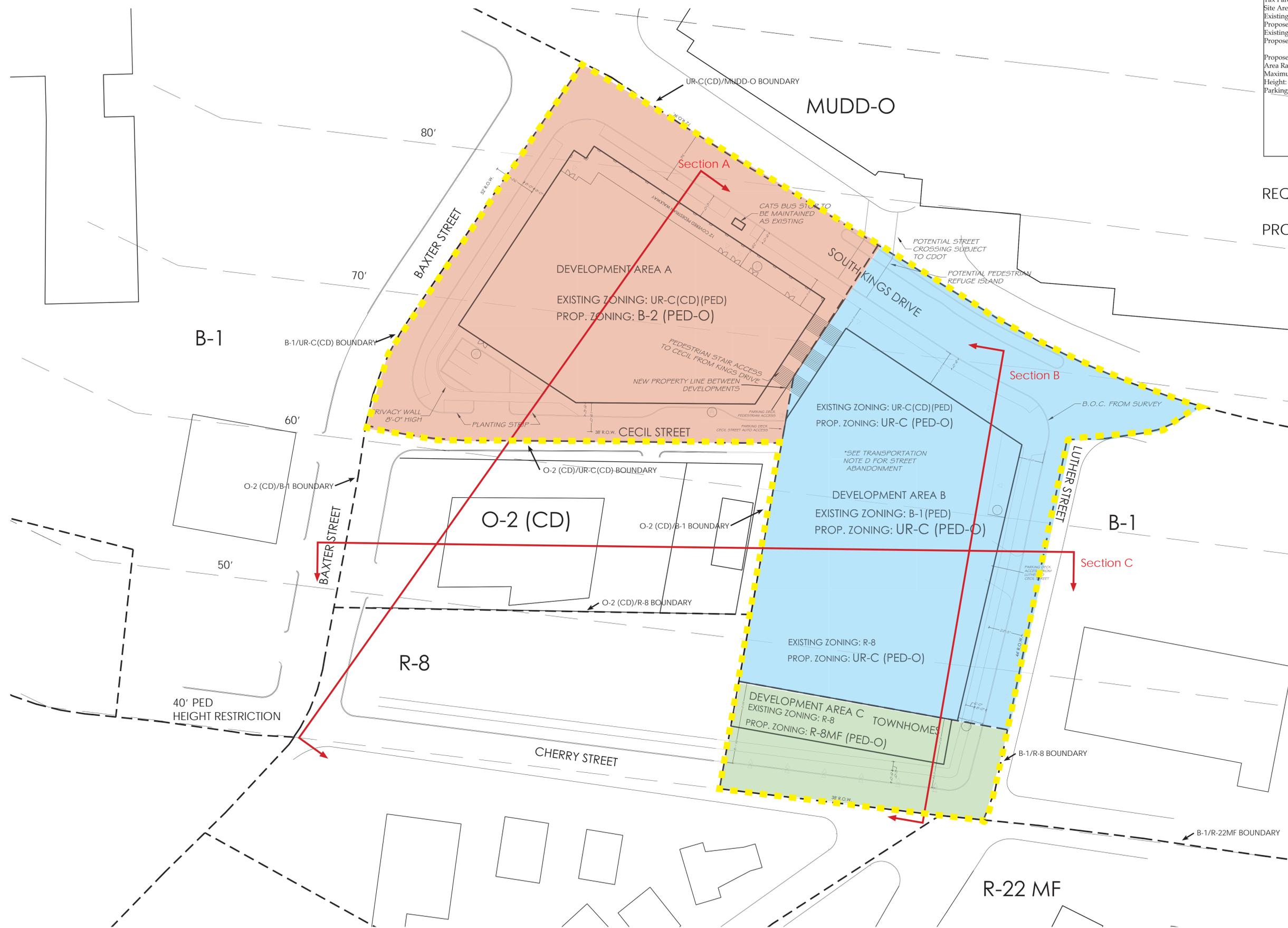
A.G. ZOUTEWELLE
SURVEYORS
1418 East Fifth St. Charlotte, NC 28204
Phone: 704-372-9444 Fax: 704-372-9555
Firm Licensure Number C-1054

*** CAUTION ***
THERE MAY BE UTILITIES OTHER THAN THOSE SHOWN.
THE SURVEYOR ASSUMES NO RESPONSIBILITY FOR UTILITIES
NOT SHOWN HEREON. IT IS THE CONTRACTOR'S
RESPONSIBILITY TO VERIFY THEIR LOCATIONS.

CALL BEFORE YOU DIG
1-800-632-4949



Site Data Table	
Tax Parcel Nos.:	125-228-01, 125-221-02, 125-221-03 and 125-221-99
Site Area:	Approximately 1.99 acres
Existing Zoning:	UR-C (CD) (PED), B-1 (PED) and R-8
Proposed Zoning:	B-2 (PED-O), UR-C (PED-O) and R-8 MF (PED-O)
Existing Uses:	Parking and single family residential
Proposed Uses:	A mixed use development that could contain office, retail, hotel and residential uses
Proposed Floor Area Ratio:	.709
Maximum Building Height:	See the Development Standards
Parking:	Per the Ordinance (Subject to Optional Provision)

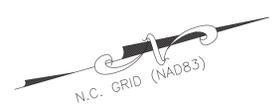


REQUIRED OPEN SPACE" 3,000 SF
 PROVIDED OPEN SPACE : 15,858 SF

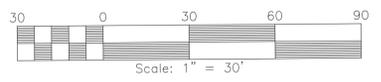
- KINGS DRIVE SETBACK 28' TO BACK OF ARCADE
16' SETBACK TO DRIP LINE
24' SETBACK TO PARKING DECK
- BAXTER STREET SETBACK 16'
- LUTHER STREET SETBACK 16'
- CHERRY STREET SETBACK 16' to 24'
- CECIL STREET CONVERTED TO PRIVATE STREET

- Development Area A
- Development Area B
- Development Area C

REZONING SITE BOUNDARY



LEGEND	
	KINGS STREET GRADE ENTRY
	CECIL STREET GRADE ENTRY
	TOWNHOME ENTRY
	FIRE EXIT
	ZONE BOUNDARY



KINGS DRIVE MIXED USE PROJECT

SITE PLAN

25 SEPT 2015

GOODE PROPERTIES



RZ1.1

DEVELOPMENT STANDARDS

September 23, 2015

GENERAL PROVISIONS

- A. These Development Standards form a part of the Rezoning Plan associated with the Rezoning Petition filed by Mktown Area Partners II, LLC to accommodate the development of a mixed use project that could contain, among other uses, office, retail, hotel and residential uses on that approximately 1.99 acre site generally bounded by South Kings Drive, Luther Street, Cherry Street and Baxter Street, which site is more particularly depicted on the Rezoning Plan (hereinafter referred to as the "Site"). The Site is comprised of those parcels of land designated as Tax Parcel Nos. 125-228-01, 125-221-02, 125-221-03 and 125-221-99.
- B. The development of the Site will be governed by the Rezoning Plan, these Development Standards and the applicable provisions of the City of Charlotte Zoning Ordinance (the "Ordinance"). Subject to the optional provisions set out below, the regulations established under the Ordinance for the B-2 (PED-O) zoning district shall govern the development of that portion of the Site designated as Development Area A on the Rezoning Plan, the regulations established under the Ordinance for the UR-C (PED-O) zoning district shall govern the development of that portion of the Site designated as Development Area B on the Rezoning Plan and the regulations established under the Ordinance for the R-8 MF (PED-O) zoning district shall govern the development of that portion of the Site designated as Development Area C on the Rezoning Plan.
- C. The development and uses depicted on the Rezoning Plan are schematic in nature and are intended to depict the general arrangement of uses and improvements on the Site. Accordingly, the ultimate layout, locations and sizes of the development and site elements depicted on the Rezoning Plan are graphic representations of the proposed development and site elements, and they may be altered or modified in accordance with any setback and yard requirements set forth on this Rezoning Plan and the development standards, provided, however, that any such alterations and modifications shall not materially change the overall design intent depicted on the Rezoning Plan.
- D. The parcels of land that comprise the Site may be recombined at the option of the Petitioner or further subdivided.
- E. The Site shall be considered to be a planned/unified development. Therefore, side and rear yards, buffers, building height separation requirements, building separation requirements and other zoning standards shall not be required internally between improvements and uses on the Site. Subject to the optional provisions set out below, the Site shall be required to meet any applicable side and rear yard requirements and buffer requirements with respect to the exterior boundary of the Site.

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- F. Pursuant to Section 1.110 of the Ordinance and Section 160A-385.1 of the North Carolina General Statutes, the Rezoning Plan, if approved, shall be vested for a period of 5 years due to the size and phasing of the development, the level of investment, economic cycles and market conditions.
- G. Future amendments to the Rezoning Plan and/or these Development Standards may be applied for by the then owner or owners of the Site in accordance with the provisions of Chapter 6 of the Ordinance. Alterations to the Rezoning Plan are subject to Section 6.207 of the Ordinance.

OPTIONAL PROVISIONS

The following optional provisions shall apply to the development of the Site:

- A. The building to be located on that portion of the Site designated as Development Area A on the Rezoning Plan shall have a maximum height of 100 feet as provided below and as depicted on the Rezoning Plan.
- B. The building/structured parking facility to be located on that portion of the Site designated as Development Area B on the Rezoning Plan shall have a maximum height ranging from 64 feet to 81 feet as provided below and as depicted on the Rezoning Plan.
- C. The single family attached dwelling units to be located on that portion of the Site designated as Development Area C on the Rezoning Plan shall have a maximum height of 48 feet as depicted on the Rezoning Plan.
- D. On-street parking along the Site's frontage on South Kings Drive shall not be required.
- E. The minimum setback from South Kings Drive for the ground floor of the building to be located on Development Area A shall be 24 feet from the back of the existing curb. The minimum setback from South Kings Drive for all floors of such building above the ground floor shall be 16 feet from the back of the existing curb.
- F. The structural support columns for the building to be located on Development Area A may be located within the setback from South Kings Drive, provided, however, that the structural support columns must be located a minimum of 16 feet from the back of the existing curb.
- G. The minimum setback from South Kings Drive for the building/structured parking facility to be located on Development Area B shall be 22 feet from the back of the existing curb.
- H. The setback, planting strip and sidewalk located along the Site's frontage on Luther Street that is more particularly depicted on the Rezoning Plan shall be permitted.

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- I. The setback, planting strip and sidewalk located along the Site's frontage on Cecil Street that is more particularly depicted on the Rezoning Plan shall be permitted.
- J. An 8 foot planting strip and an 8 foot sidewalk shall be installed along the Site's frontage on Baxter Street rather than an 8 foot planting strip, a 6 foot sidewalk and a 2 foot utility zone.
- K. The off-street parking requirement for eating, drinking and entertainment establishments (Type 1 and Type 2) shall be 1 parking space per 600 square feet of gross floor area.
- L. Surface parking and vehicular maneuvering areas shall be permitted between the building to be located on Development Area A and Cecil Street and Baxter Street as depicted on the Rezoning Plan.
- M. A roof line variation every 30 feet shall not be required for the building/structured parking facility to be located on Development Area B, and a roof line variation shall not be required every 30 feet for the single family attached dwelling units to be located on Development Area C.
- N. Tree grates, seat walls, planters and other pedestrian amenities and features may be provided in the setback along the Site's frontage on South Kings Drive.
- O. The 10 foot buffer required along the relevant portions of the Site's southern boundary line under Section 10.812(5) of the Ordinance shall not be required.
- P. The side and rear yard requirements of Section 10.812(1) of the Ordinance shall not be required along the Site's southern boundary line.

PERMITTED USES

A. Development Area A

The building to be located on Development Area A may only be devoted to the uses set out below (including any combination of such uses), together with any incidental or accessory uses associated therewith that are permitted under the Ordinance in the B-2 (PED-O) zoning district:

- (1) Multi-family dwelling units and planned multi-family dwelling units;
- (2) Dwellings, mixed use;
- (3) Eating, drinking and entertainment establishments (Type 1), and eating, drinking and entertainment establishments (Type 2) subject to the applicable regulations of Section 12.546 of the Ordinance;
- (4) Hotels;
- (5) Art galleries;

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- (6) Colleges, universities, commercial schools and schools providing adult training in any of the sciences, trades and professions;
- (7) Professional business and general offices;
- (8) Retail sales;
- (9) Services, including, without limitation, beauty shops and barbershops, spas and fitness centers;
- (10) Private postal services; and
- (11) Studios for artists, designers, photographers, musicians, sculptors, gymnasts, potters, wood and leather craftsmen, glass blowers, weavers, silversmiths, and designers of ornamental and precious jewelry.

B. Development Area B

The building to be located on Development Area B shall be devoted primarily to a structured parking facility to serve the uses located on Development Area A and Development Area C and to any incidental or accessory uses relating thereto as depicted on the Rezoning Plan.

Notwithstanding the foregoing, that portion of the ground or street level floor of the building to be located on Development Area B facing South Kings Drive that is more particularly depicted on the Rezoning Plan may only be devoted to the uses set out below (including any combination of such uses), together with any incidental or accessory uses relating thereto that are permitted under the Ordinance in the UR-C (PED-O) zoning district:

- (1) Eating, drinking and entertainment establishments (Type 1), and eating, drinking and entertainment establishments (Type 2) subject to the applicable regulations of Section 12.546 of the Ordinance;
- (2) Art galleries;
- (3) Professional business and general offices;
- (4) Retail sales;
- (5) Services, including, without limitation, beauty shops and barbershops, spas and fitness centers; and
- (6) Studios for artists, designers, photographers, musicians, sculptors, gymnasts, potters, wood and leather craftsmen, glass blowers, weavers, silversmiths, and designers of ornamental and precious jewelry.

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C. Development Area C

That portion of the Site designated as Development Area C on the Rezoning Plan may be devoted only to a maximum of 8 single family attached dwelling units, together with any incidental or accessory uses associated therewith that are permitted under the Ordinance in the R-8 MF (PED-O) zoning district.

Development Limitations/Requirements

- A. The maximum gross floor area of the building to be located on Development Area A shall be 275,000 square feet, and the maximum height of this building shall be 100 feet as more particularly depicted on the Rezoning Plan.
- B. Notwithstanding anything contained herein to the contrary, the maximum number of multi-family dwelling units that may be located in the building to be located on Development Area A shall be 300 dwelling units.
- C. Notwithstanding anything contained herein to the contrary, the maximum number of hotel rooms that may be located in the building to be located on Development Area A shall be 225 hotel rooms.
- D. Ground floor uses located in the building to be located on Development Area A along South Kings Drive may only be devoted to non-residential uses, and the primary entrances into such uses will be oriented to and at street grade with South Kings Drive.
- E. The maximum gross floor area of the building/structured parking facility to be located on Development Area B shall be 221,000 square feet, and the maximum height of the building/structured parking facility shall range from 64 feet to 81 feet as depicted on the Rezoning Plan. The gross floor area of the commercial (non-residential) uses to be located on the ground or street level of the building/structured parking facility located on Development Area B facing South Kings Drive shall not be considered or counted when determining the maximum gross floor area of the building/structured parking facility.
- F. The maximum gross floor area of the commercial (non-residential) uses to be located on the ground or street level of the building/structured parking facility located on Development Area B facing South Kings Drive shall be 7,000 square feet.
- G. The uses located on the ground or street level floor of the building/structured parking facility to be located on Development Area B facing South Kings Drive may only be devoted to non-residential uses, and the primary entrances into such uses will be oriented to and at street grade with South Kings Drive. The remaining portions of the ground or street level floor of this building/structured parking facility may only be devoted to the structured parking facility.
- H. Single family attached dwelling units are required to be constructed on Development Area C along its frontage on Cherry Street adjacent to the building/structured parking facility to be constructed on Development Area B as generally depicted on the Rezoning

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Plan to provide a residential edge to this portion of the Site. The maximum number of single family attached dwelling units that may be constructed on Development Area C shall be 8 dwelling units.

- I. The single family attached dwelling units to be located on Development Area C shall be constructed prior to the issuance of a certificate of occupancy for the building/structured parking facility to be located on Development Area B.

TRANSPORTATION

- A. Vehicular access shall be as generally depicted on the Rezoning Plan. The placement and configuration of the access points are subject to any minor modifications required by the Charlotte Department of Transportation.
- B. Subject to the optional provisions set out above, off-street vehicular parking shall meet the minimum requirements of the Ordinance.
- C. Bicycle parking will be provided on the Site as required by the Ordinance.
- D. Petitioner has submitted a Petition to the City of Charlotte requesting that Cecil Street be abandoned by the City of Charlotte. In the event that this Petition is approved by City Council, Cecil Street shall be converted to a private drive serving the adjacent parcels of land, the building to be located on Development Area A and the structured parking facility to be located on Development Area B.
- E. In the event that Cecil Street is abandoned and converted to a private street, a public access easement shall be provided to allow vehicular traffic to utilize this street to travel to and from Baxter Street from and to Luther Street.
- F. The existing bus shelter located along the Site's frontage on South Kings Drive shall be retained provided, however, that with the approval of CATS and CDOT, the existing bus shelter may be relocated by the Petitioner to another location along the Site's frontage on South Kings Drive.
- G. Direct vehicular access from Luther Street to Cecil Street through the structured parking facility located on Development Area B shall be provided.

ARCHITECTURAL AND DESIGN STANDARDS

- A. The maximum height of the building to be constructed on Development Area A shall be 100 feet as more particularly depicted on the Rezoning Plan.
- B. The maximum height of the building/structured parking facility to be located on Development Area B shall range from 64 feet to 81 feet as more particularly depicted on the Rezoning Plan.

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- C. The maximum height of the single family attached dwelling units to be constructed on Development Area C shall be 48 feet as depicted on the Rezoning Plan.

- D. All roof mounted mechanical equipment will be screened from view from adjoining public rights-of-way and abutting properties as viewed from grade.
- E. Dumpster and recycling areas will be enclosed on all four sides by an opaque wall or fence with one side being a hinged opaque gate. If one or more sides of a dumpster and recycling area adjoin a side wall or rear wall of a building, then the side wall or rear wall of the building may be substituted for the wall or fence along each such side. Alternatively, such facilities may be located within or under the structures proposed to be constructed on Development Area A and Development Area B.
- F. Attached to the Rezoning Plan are conceptual, schematic images of the various exterior components and elements of the building and improvements to be constructed on Development Area A that are intended to depict the general conceptual architectural style, character and quality of such building and improvements. Accordingly, the building and improvements to be constructed on Development Area A shall be designed and constructed so that the exterior components and elements thereof are substantially similar in appearance to the attached conceptual, schematic images with respect to architectural style, character and quality. Notwithstanding the foregoing, changes and alterations to the exterior components and elements of the building and improvements to be constructed on Development Area A which do not materially change the overall conceptual architectural style, character and quality shall be permitted.

- G. In addition to the design flexibility provided in paragraph F above, the exterior design and exterior building materials of the building and improvements to be located on Development Area A may be modified to accommodate the various uses that are permitted on the Site under this Rezoning Plan, which modifications may include, without limitation, the addition of balconies, overhangs, recesses and modulations and the use of alternative exterior building materials.

- H. Attached to the Rezoning Plan are conceptual, schematic images of the various exterior components and elements of the building/structured parking facility to be constructed on Development Area B that are intended to depict the general conceptual architectural style, character and quality of such building/structured parking facility. Accordingly, the building/structured parking facility to be constructed on Development Area B shall be designed and constructed so that the exterior components and elements thereof are substantially similar in appearance to the attached conceptual, schematic images with respect to architectural style, character and quality. Notwithstanding the foregoing, changes and alterations to the exterior components and elements of the building/structured parking facility to be constructed on Development Area B which do not materially change the overall conceptual architectural style, character and quality shall be permitted.

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- I. Attached to the Rezoning Plan are conceptual, schematic images of the various exterior components and elements of the single family attached dwelling units to be constructed on Development Area C that are intended to depict the general conceptual architectural style, character and quality of the single family attached dwelling units. Accordingly, the single family attached dwelling units to be constructed on Development Area C shall be designed and constructed so that the exterior components and elements thereof are substantially similar in appearance to the attached conceptual, schematic images with respect to architectural style, character and quality. Notwithstanding the foregoing, changes and alterations to the exterior components and elements of the single family attached dwelling units to be constructed on Development Area C which do not materially change the overall conceptual architectural style, character and quality shall be permitted.

- J. Each end of the building containing the single family attached dwelling units will have a combination of windows, doors or other architectural details on each floor to break up blank walls and to provide architectural interest.

SETBACK AND YARDS/STREETScape

- A. Subject to the optional provisions set out above, the development of the Site shall comply with the setback, side yard and rear yard requirements of the PED overlay zoning district.
- B. Subject to the optional provisions set out above, Petitioner shall install planting strips and sidewalks along the Site's public street frontages as generally depicted on the Rezoning Plan.
- C. Tree grates, seat walls, planters and other pedestrian amenities and features may be provided in the setback along the Site's frontage on South Kings Drive.

ENVIRONMENTAL FEATURES

- A. Development of the Site shall be in compliance with the Charlotte City Council approved and adopted Post Construction Controls Ordinance.
- B. The location, size, and type of storm water management systems are subject to review and approval as part of the full development plan submittal and are not implicitly approved with this rezoning. Adjustments may be necessary in order to accommodate actual storm water treatment requirements and natural site discharge points.
- C. Development of the Site shall comply with the City of Charlotte Tree Ordinance.

SIGNS

All signs installed on the Site shall comply with the requirements of the Ordinance.

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LIGHTING

- A. All freestanding lighting fixtures installed on the Site (excluding street lights and lower, decorative lighting that may be installed along the driveways, sidewalks and parking areas) shall be fully capped and shielded and the illumination downwardly directed so that direct illumination does not extend past any property line of the Site. The maximum height of any freestanding lighting fixtures installed on the Site shall be 20 feet.
- B. Any lighting fixtures attached to the structures to be constructed on the Site shall be decorative, capped and downwardly directed.

BINDING EFFECT OF THE REZONING DOCUMENTS AND DEFINITIONS

- A. If this Rezoning Petition is approved, all conditions applicable to the use and development of the Site imposed under these Development Standards and the Rezoning Plan will, unless amended in the manner provided under the Ordinance, be binding upon and inure to the benefit of Petitioner and the current and subsequent owners of the Site and their respective successors in interest and assigns.
- B. Throughout these Development Standards, the term "Petitioner" shall be deemed to include the heirs, devisees, personal representatives, successors in interest and assigns of Petitioner or the owner or owners of the Site from time to time who may be involved in any future development thereof.
- C. Any reference to the Ordinance herein shall be deemed to refer to the requirements of the Ordinance in effect as of the date this Rezoning Petition is approved.

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STREET-LEVEL RETAIL

PEDESTRIAN CONNECTION TO CECIL STREET

PEDESTRIAN RETAIL COLONNADE

GOODE PROPERTIES

MIDTOWN CENTER
KINGS DRIVE ELEVATION



100'

65'

128'

PARKING DECK
KINGS DRIVE RETAIL AT BASE

190'

RETAIL - OFFICE - HOTEL - RESIDENTIAL
FIRST LEVEL RETAIL & KINGS DRIVE COLONNADE

--- BUILDING CORNICE
DENOTES TOP OF BUILDING.

--- HEIGHT AND DEPTH OF
COLUMNS RECEDES
INWARD TO DISSOLVE
BUILDING PROFILE AGAINST
SKY.

--- BELT COURSES ARTICULATE
EACH LEVEL.

--- BUILDING BASE
ARTICULATED BY
PEDESTRIAN COLONNADE.

GOODE PROPERTIES

MIDTOWN CENTER
ELEVATION GUIDE



MIDTOWN CENTER
PEDESTRIAN ACCESS FROM KINGS DRIVE TO CECIL STREET

GOODE PROPERTIES



MIDTOWN CENTER
VIEW OF STREET-GRADE PEDESTRIAN &
RETAIL COLONNADE

GOODE PROPERTIES





GOODE PROPERTIES

MIDTOWN CENTER
VIEW OF PARKING DECK + RETAIL
AT KINGS DRIVE AND LUTHER



GOODE PROPERTIES

DESIGN INTENTIONS

- BELT COURSES AND MOLDING ARTICULATE EACH LEVEL OF BUILDING HEIGHT.
- FURTHER DIFFERENTIATION OF STRING-COURSE HEIGHT+DEPTH AND COLUMN WIDTH+DEPTH ARTICULATES UPPER STORIES AS DISCRETE PARTS OF BUILDING FORM
- 15 STORY BUILDING BASE SCALED TO PEDESTRIAN STREET ACTIVITY.
- PEDESTRIAN ARCADE (10' DEEP) GIVES PEDESTRIAN SPACE THAT EXCEEDS MINIMUM SETBACK REQUIREMENTS AND ORDINANCE RECOMMENDATIONS.
- ARCHITECTURAL CANOPY ACCENTS 'CEILING' OF PEDESTRIAN SPACE



GOODE PROPERTIES

MIDTOWN CENTER
BAXTER STREET ELEVATION





CUSTOMER ENTRY TO RETAIL BELOW

STONE CLADDING FIRST LEVEL

OFFICE ENTRY & PORTE-COCHERE

PEDESTRIAN CONNECTION DOWN TO KINGS DRIVE

MIDTOWN CENTER
CECIL STREET ELEVATION
OCTOBER 2014



GOODE PROPERTIES

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MIDTOWN CENTER
VIEW OF OFFICE AND PARKING DECK
AT CECIL STREET



GOODE PROPERTIES



MIDTOWN CENTER
VIEW OF PARKING DECK + RETAIL
AT UPPER KINGS DRIVE



GOODE PROPERTIES



FOUNDATION SURFACES TO BE CLAD IN STONE WITH FOLIAGE TO COVER

STREET-GRADE RETAIL ALONG KINGS DRIVE

MIDTOWN CENTER PARKING DECK
VIEW ALONG LUTHER STREET

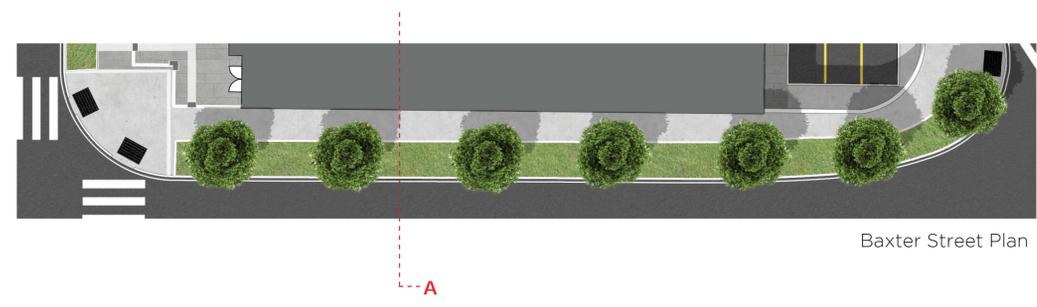
GOODE PROPERTIES

15



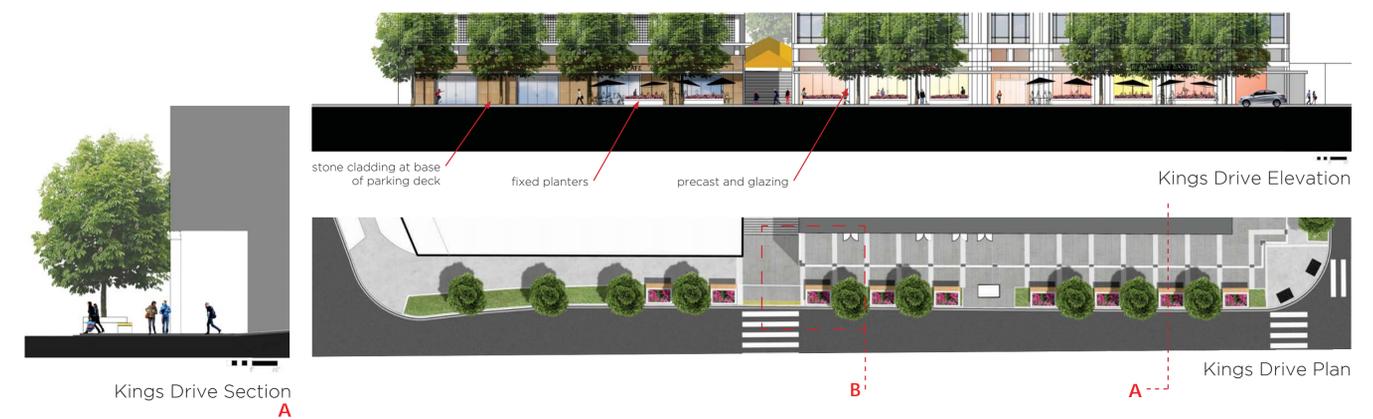


Street Section A
Baxter Street Elevation



Baxter Street Plan

South Kings Drive
Mixed Use Project
Supplemental Document
WAGNERMURRAY ARCHITECTS



Kings Drive Section A
Kings Drive Plan



Ground Plan Enlargement B

South Kings Drive
Mixed Use Project
Supplemental Document
WAGNERMURRAY ARCHITECTS

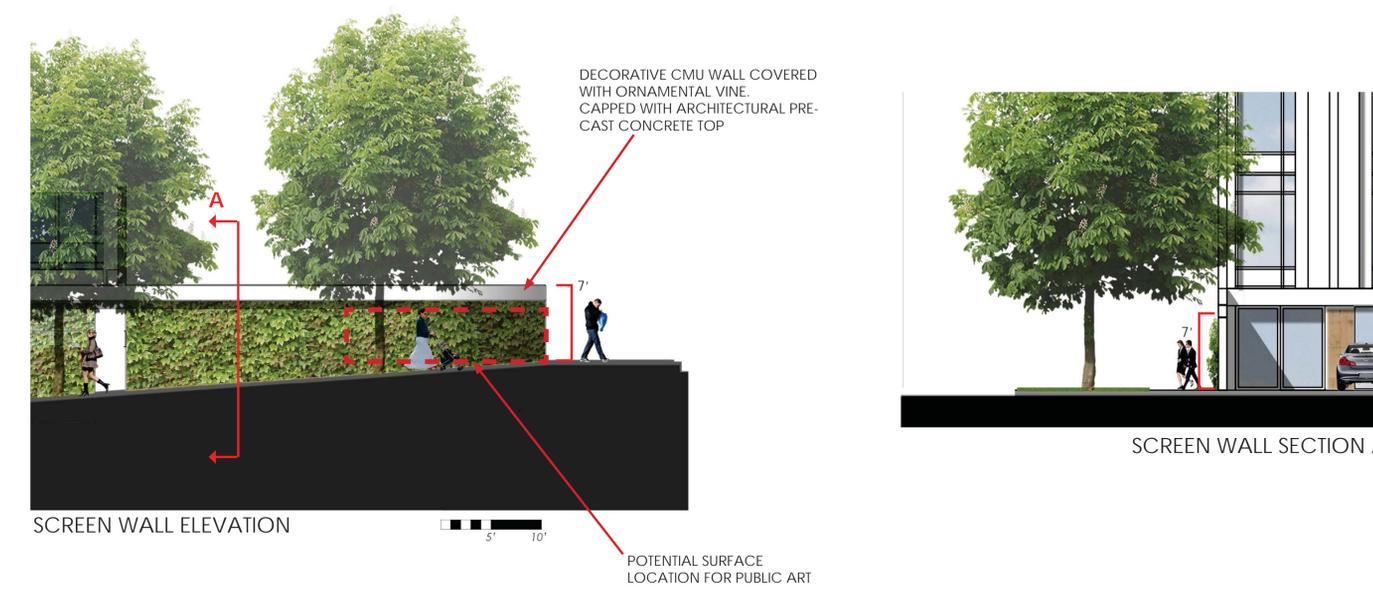


Cecil Street Elevation



Cecil Street Plan

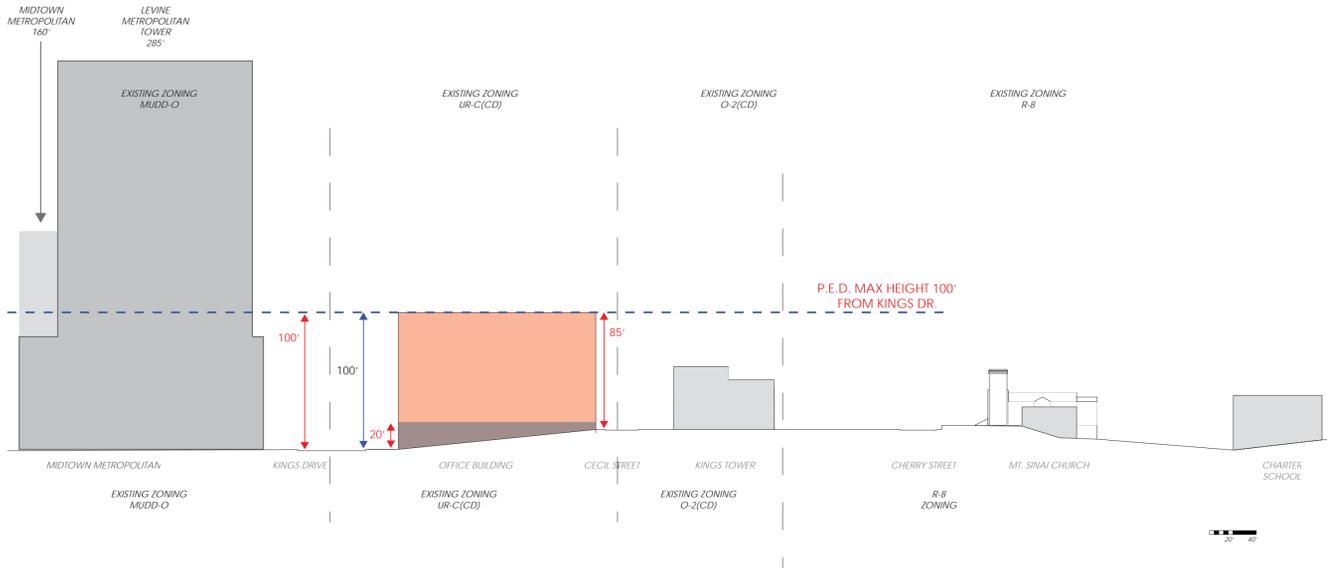
South Kings Drive
Mixed Use Project
Supplemental Document
WAGNERMURRAY ARCHITECTS



SCREEN WALL ELEVATION

SCREEN WALL SECTION A

South Kings Drive
Mixed Use Project
Supplemental Document
WAGNERMURRAY ARCHITECTS



*BUILDING WILL HAVE MAXIMUM HEIGHT OF 119'

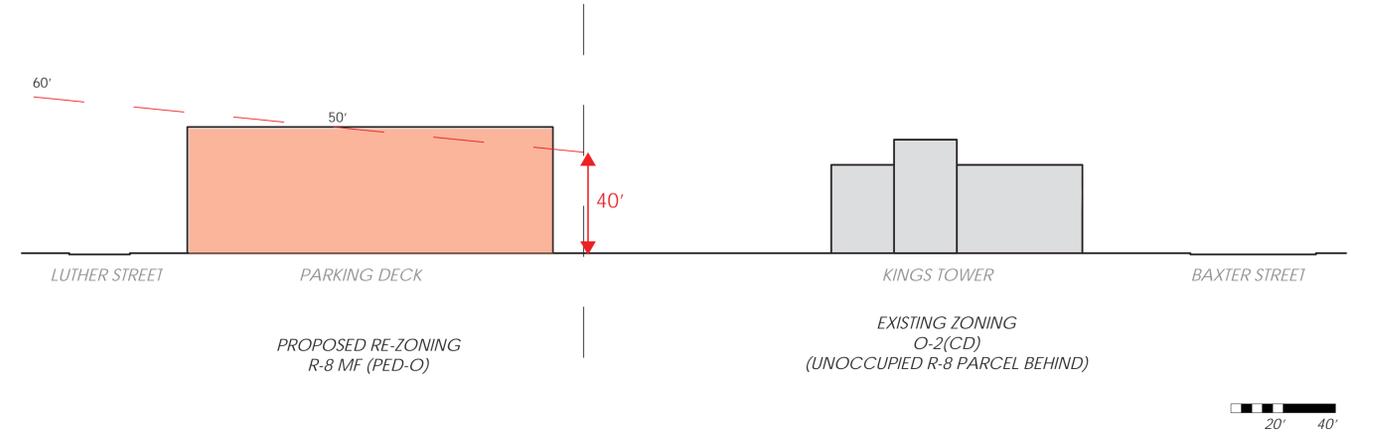
ALL BUILDINGS HEIGHTS PER SECTION 2.201

REVISED

GOODE PROPERTIES

REVISED PED OVERLAY DISTRICT ANALYSIS
100' HEIGHT
DEVELOPMENT PARCEL A

4



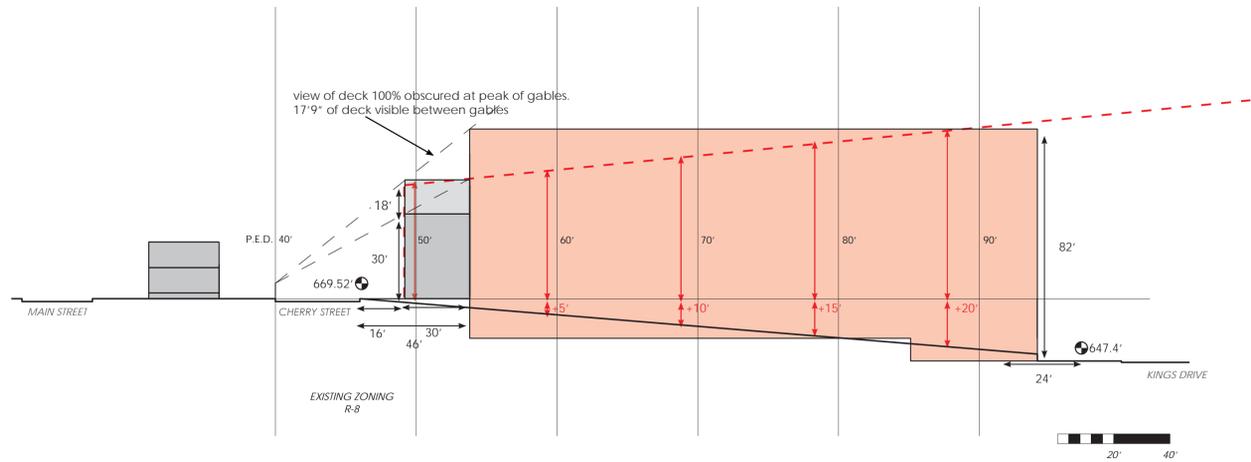
*BUILDING WILL HAVE MAXIMUM HEIGHT OF 50'

ALL BUILDINGS HEIGHTS PER SECTION 2.201

GOODE PROPERTIES

REVISED PED OVERLAY DISTRICT ANALYSIS
9' HEIGHT VARIANCE
DEVELOPMENT PARCEL B

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GOODE PROPERTIES

REVISED PED OVERLAY DISTRICT ANALYSIS
LUTHER STREET SECTION
DEVELOPMENT PARCEL B

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THE ILLUSTRATIVE ELEVATIONS ARE PROVIDED TO REFLECT THE ARCHITECTURAL STYLE AND QUALITY OF THE BUILDING THAT MAY BE CONSTRUCTED AND THE ACTUAL BUILDING ON THE SITE MAY VARY FROM THE ARCHITECTURAL CONCEPT ILLUSTRATIONS AS LONG AS THE GENERAL ARCHITECTURAL CONCEPTS AND INTENT ILLUSTRATED ARE MAINTAINED. THE DEVELOPMENT DEPICTED IN THE SITE AND BUILDING PLANS IS SCHEMATIC IN NATURE, INTENDED TO DESCRIBE THE GENERAL LAYOUT AND ARRANGEMENT OF THE BUILDING AND MAY BE ALTERED OR MODIFIED DURING DESIGN DEVELOPMENT AND CONSTRUCTION PHASES SUBJECT TO LOCAL AREA ZONING REQUIREMENTS. THE DESIGN OF CERTAIN FEATURES MAY VARY DEPENDING ON FINAL DRAWINGS AND ON ENVIRONMENTAL AND REGULATORY MATTERS.



SCHEMATIC BUILDING ELEVATION

Goode Properties

BAXTER STREET at MIDTOWN CENTER
CHARLOTTE, NC
April 2, 2015



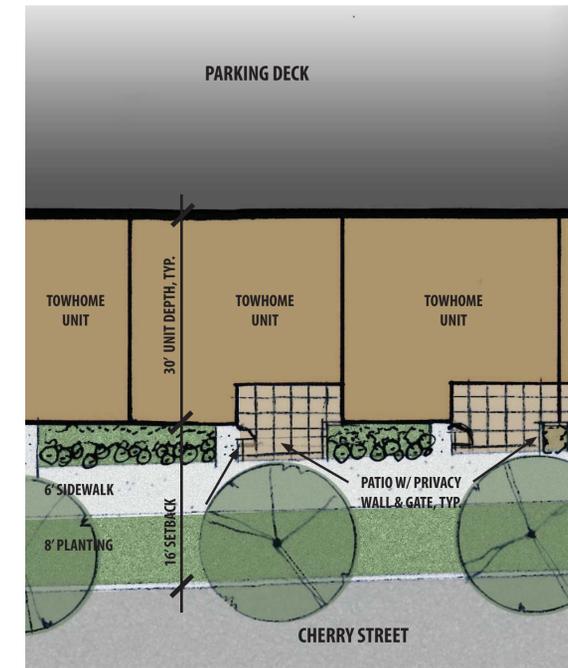
THE ILLUSTRATIVE ELEVATIONS ARE PROVIDED TO REFLECT THE ARCHITECTURAL STYLE AND QUALITY OF THE BUILDING THAT MAY BE CONSTRUCTED AND THE ACTUAL BUILDING ON THE SITE MAY VARY FROM THE ARCHITECTURAL CONCEPT ILLUSTRATIONS AS LONG AS THE GENERAL ARCHITECTURAL CONCEPTS AND INTENT ILLUSTRATED ARE MAINTAINED. THE DEVELOPMENT DEPICTED IN THE SITE AND BUILDING PLANS IS SCHEMATIC IN NATURE, INTENDED TO DESCRIBE THE GENERAL LAYOUT AND ARRANGEMENT OF THE BUILDING AND MAY BE ALTERED OR MODIFIED DURING DESIGN DEVELOPMENT AND CONSTRUCTION PHASES SUBJECT TO LOCAL AREA ZONING REQUIREMENTS. THE DESIGN OF CERTAIN FEATURES MAY VARY DEPENDING ON FINAL DRAWINGS AND ON ENVIRONMENTAL AND REGULATORY MATTERS.



SCHEMATIC BUILDING MATERIALS STUDY

Goode Properties

BAXTER STREET at MIDTOWN CENTER
CHARLOTTE, NC
April 2, 2015



PARTIAL CONCEPTUAL SITE PLAN

Goode Properties

BAXTER STREET at MIDTOWN CENTER
CHARLOTTE, NC
April 2, 2015



THE ILLUSTRATIVE ELEVATIONS ARE PROVIDED TO REFLECT THE ARCHITECTURAL STYLE AND QUALITY OF THE BUILDING THAT MAY BE CONSTRUCTED AND THE ACTUAL BUILDING ON THE SITE MAY VARY FROM THE ARCHITECTURAL CONCEPT ILLUSTRATIONS AS LONG AS THE GENERAL ARCHITECTURAL CONCEPTS AND INTENT ILLUSTRATED ARE MAINTAINED. THE DEVELOPMENT DEPICTED IN THE SITE AND BUILDING PLANS IS SCHEMATIC IN NATURE, INTENDED TO DESCRIBE THE GENERAL LAYOUT AND ARRANGEMENT OF THE BUILDING AND MAY BE ALTERED OR MODIFIED DURING DESIGN DEVELOPMENT AND CONSTRUCTION PHASES SUBJECT TO LOCAL AREA ZONING REQUIREMENTS. THE DESIGN OF CERTAIN FEATURES MAY VARY DEPENDING ON FINAL DRAWINGS AND ON ENVIRONMENTAL AND REGULATORY MATTERS.



SCHEMATIC SIDE ELEVATION AT LUTHER STREET

Goode Properties

BAXTER STREET at MIDTOWN CENTER
CHARLOTTE, NC
April 2, 2015



Attachment A

Introduction

The Interstate-85 (I-85) North Bridge project provides an east-west connection between the University Research Park and the LYNX J.W. Clay Boulevard Station, the Charlotte Research Institute, the UNCC Campus, and the shopping and services at University Place. The project will construct a bridge over I-85 connecting Research Drive to JW Clay Boulevard, just north of the Harris Boulevard interchange with I-85. The intended project delivery method is Design-Build (D-B), which is a proven transportation method throughout the country.

Differences between Design-Build and traditional Design-Bid-Build

Staff recommends using the design-build (D-B) project delivery method on this project, rather than traditional design-bid-build (DBB). In traditional design-bid-build, the City contracts with an engineer to produce complete design plans, which are then bid out to contractors. The contractor with the lowest responsive bid is then awarded the construction contract. There are two separate contracts involved (engineer, contractor), and the contract price is set only after bidding is complete.

On a design-build project, the process begins with a preliminary design to capture the key project requirements. Project teams made up of a design firm and a contractor submit competing proposals for the best overall concept and cost for the project. The selected team is then contracted to complete the design and build the project for the amount that they proposed. The design firm that did the initial preliminary design can be retained as owner's representative (as is being proposed on this project) to verify that the project meets the design intents required by the original concept.

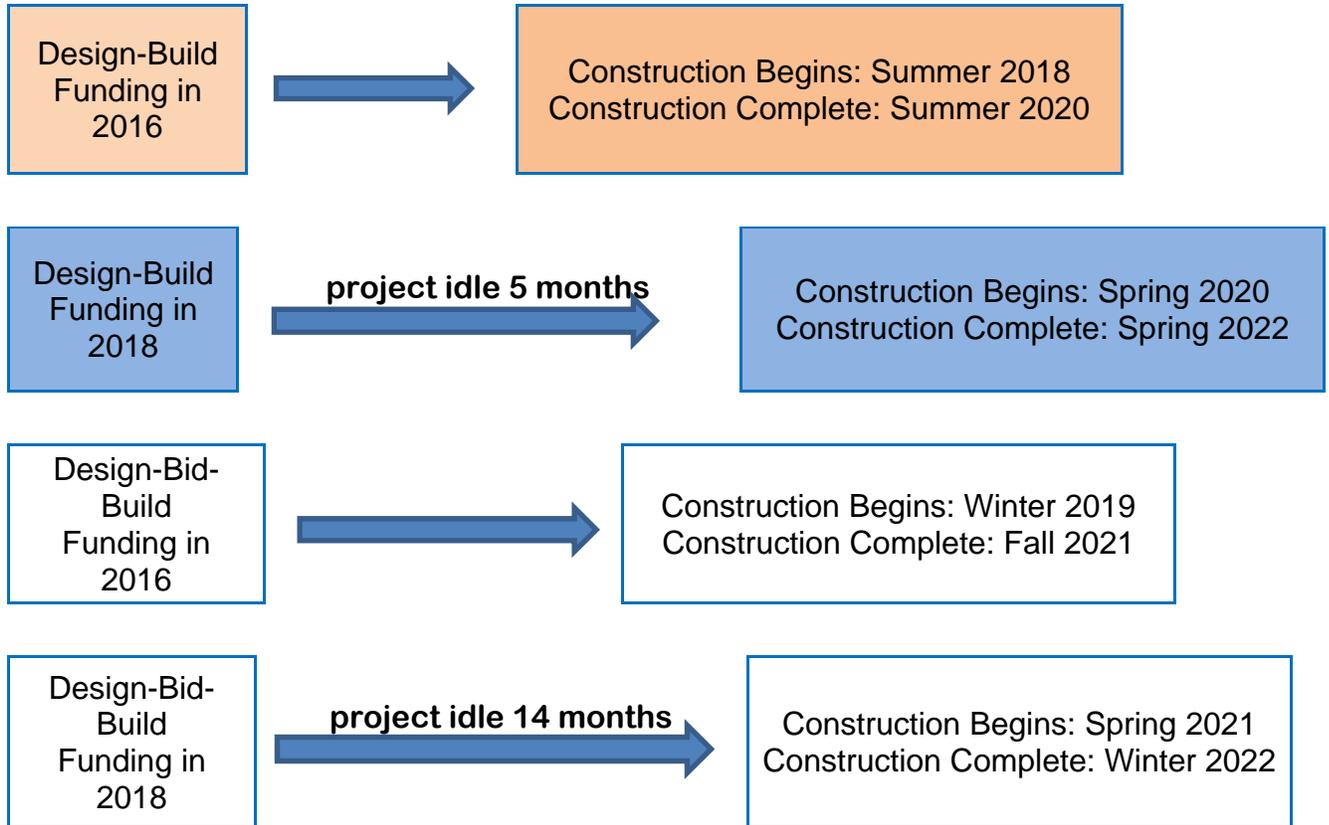
All projects, regardless of delivery method, involve planning, design, real-estate acquisition, utility relocation, and construction. In DBB, these phases occur sequentially. By contrast, in a design-build framework, the design, real estate, utility relocation, and construction phases can occur concurrently to some degree, allowing for shorter project durations

Why Design Build

Staff believes that this project is a good candidate for design build because of the project size, limited right of way needed and the opportunity for an innovative design proposal either for structures or constructability. Close involvement of a contractor with a design partner creates an opportunity for this kind of innovation.

Delivery Method, Schedule, and Funding

The North Bridge schedule can vary significantly based on the project delivery method and the bond funding year. The chart below shows the schedule implications of each scenario.



Project Delivery Method	Funding Year	Construction Start	Construction Complete	Idle time in process
Design-Build	2016	Summer 2018	Summer 2020	no
Design-Build	2018	Spring 2020	Spring 2022	yes 5 months
Design-Bid-Build	2016	Winter 2019	Fall 2021	no
Design-Bid-Build	2018	Spring 2021	Winter 2022	yes 14 months

Blue shading: current funding plan and project delivery method

Orange shading: Staff's recommended funding plan and project delivery method.

The decision of which project delivery method to use – design-build versus design-bid-build – can be made independently from deciding in which year to fund the project. The North Bridge project would make for a good design-build project regardless of funding year. The contract on the Council agenda for 9/28/15 for award to HDR Engineering is set up to move the project forward as a design-build transportation project, but can be adjusted at a later date if necessary. The planning, environmental documentation, and community engagement components of the contract have to be done regardless of funding year or delivery method. The project team would begin activities related to the design-build process in May 2016, but can be pushed out if the funding schedule remains.

Larger Context of the CIP

Staff recommends the first option above (implement as a design-build project and advance the funding from the 2018 bond to the 2016 bond), recognizing that the I-85 North Bridge is only one project in the City's larger Community Investment Program, and that it cannot be looked at in isolation. Approving the Planning/Owner's Advisor contract at the September 28 Council meeting allows for the project to begin planning but does not commit the City to accelerating it. Through the fall of 2015 and winter/spring of 2016, staff expects to present to the Transportation and Planning Committee and/or the full City Council a series of updates on the CIP, funding assumptions, delivery methods, and any recommended changes to individual projects.

47. LYNX Blue Line Extension Sugar Creek Parking Garage

Action:

Award a construction contract to the lowest, responsive bidder, Messer Construction, for the construction of a Parking Garage and Pedestrian Bridge at the BLE Sugar Creek Station in the amount of \$23,346,850.00.

**Staff Resource(s): John Lewis, Jr. CATS
Danny Rogers, CATS**

Explanation

- The Sugar Creek Parking Garage will be located on Raleigh Street at the Blue Line Extension (BLE) Sugar Creek Station.
- The construction of this parking garage and pedestrian bridge is integral to the BLE.
- The parking deck will have four levels with 630 parking spaces and will include:
 - Three bus bays served by a private driveway adjacent to the parking garage;
 - A pedestrian bridge that will extend from the east end of the Sugar Creek Station platform, crossing over the North Carolina Rail Road right-of-way, and connecting to Bearwood Avenue; and
 - Elevators at both ends of the pedestrian bridge.
- On June 17, 2015, CATS advertised an Invitation To Bid (ITB) for the Sugar Creek parking garage and pedestrian bridge; five bids were received from interested service providers.
- Archer-Western Construction, LLC was the apparent low bidder with a bid of \$22,726,999.63.
- Messer Construction was the lowest responsive bidder with a bid of \$23,346,850.00.

Responsiveness

- The apparent low bidder, Archer-Western, was found by staff to be non-responsive based on the Disadvantage Business Enterprise (DBE) requirements included in the Invitation to Bid.
- Archer-Western did not meet the DBE Goal nor did it satisfy the alternative 'Good Faith Efforts' standard.
- Pursuant to federal law, a bidder must show that it made adequate Good Faith Efforts to garner the participation of certified DBE firms before it may be awarded a federally funded contract.
- In order to determine whether adequate Good Faith Efforts were made, staff reviewed the documentation provided by the bidder and considered the quality,

quantity, and intensity of the bidder's efforts. To meet the standard, the efforts employed by the bidder must be those that would be reasonably expected of a bidder that was actively and aggressively trying to obtain DBE participation.

- Additionally, bidders found to have not met the DBE requirements are afforded an opportunity for an administrative reconsideration.
- An administrative reconsideration allows the bidder to meet with a separate and independent hearing officer that did not participate in the original determination. The bidder may present both documentation and argument concerning whether it met the goal or met the good faith efforts standard. The hearing officer has the discretion to set aside the original determination if merited.
- In this case, an administrative reconsideration was held and the hearing officer upheld the original determination that Archer-Western did not meet the DBE requirements.

Disadvantage Business Enterprise (DBE)

DBE participation goal was set for this contract pursuant to the City's federally required and approved DBE program.

Established DBE Goal: 7.25%

Committed DBE Goal: 7.34%

Messer Construction Co. exceeded the established DBE goal subcontracting goal, and has committed 7.34% (\$1,713,659) of the total contract amount to the following certified DBE firms:

- Roadmasters (DBE,SBE,WBE) (\$7,500) (traffic control)
- Carroll Small Repairs (DBE) (\$490,072) (drywall)
- United Painting (DBE,SBE,MBE) (\$72,580) (painting)
- RJ Leeper (DBE,MBE) (\$118,010) (management)
- Alamance Striping & Equipment (DBE) (\$8,876) (striping)
- TEC Electric (DBE,SBE,MBE) (\$1,004,621) (electrical)

Fiscal Note

Funding: CATS Community Investment Plan



Fiscal Year 2015 Performance Report

STATE OF THE CITY

Investing in Our Community

During FY2015, Charlotteans invested in the community by supporting a \$146 million bond referendum to improve three core quality of life elements: livability, getting around, and job growth. Major investments include:

- The [Cross Charlotte Multi-Use Trail](#) will be a continuous bikeway extending across Charlotte from Pineville to the Cabarrus County line. This project will provide significant transportation and economic development benefits and will connect many key destinations including Carolina Place, Park Road Shopping Center, Charlotte Center City, UNCC, and the Noda Arts and Entertainment District.
- The [Comprehensive Neighborhood Improvement Program \(CNIP\)](#) represents a new approach to capital planning in Charlotte that will look beyond the boundaries of residential areas for opportunities to enhance collaboration with public/private partners, leverage multiple investments in neighborhoods, and catalyze changes occurring in neighborhoods. The program will also coordinate with public safety, code enforcement, social services, and education to bring additional resources to the CNIP areas.
- The [Northeast Corridor Infrastructure project](#) will improve pedestrian, bicyclist, and motorist access to the CATS Blue Line Extension. This investment will help station

area plans along the Blue Line Extension as well as provide broader connections to other community investments like the Cross-Charlotte Trail and Mecklenburg County greenway system.

Transit

FY2015 marked the seventh year of [LYNX Blue Line light rail service](#) in Charlotte. Significant progress was made on the construction of the 9.3 mile Blue Line extension from Uptown to University City, which is scheduled to open in 2017.

Construction of the 1.5-mile [CityLYNX Gold Line Phase 1](#) streetcar project completed in June of 2015 with the streetcar opening to the general public on July 14. Funding for Phase 2 was included in the President's FY2016 proposed budget in the amount of \$75 million.

Competitive Tax Rates

Charlotte continues to offer high quality services at a low cost. The City of Charlotte's FY2015 [property tax rate](#) was the second lowest among the five largest North Carolina cities, and its water and sewer rates are significantly lower than the state average.

AAA Bond Rating

The City maintained its [AAA bond rating](#), the highest level possible, for general obligation debt for the 42nd consecutive year. Charlotte Water also achieved AAA bond ratings for water and sewer capital improvement debt which makes it one of only a few dozen water and sewer providers in the country

with that level of stability and performance.

Increasing Property Values and Residential Construction

Charlotte home values continue to rebound from the recession. The [Case-Shiller U.S. National Home Price Index](#) indicated that Charlotte's April 2015 home prices increased 5.59 percent over 2014 levels.

Single-family residential [building permits](#) increased 9.5 percent over FY2014 levels and the number of multifamily building permits increased by 12.8 percent over the previous fiscal year.

Quality of Life

On May 4, 2015, the City of Charlotte and Mecklenburg County released the updated [Charlotte-Mecklenburg Quality of Life Explorer](#), an interactive online tool with over 80 variables that can be used to explore social, housing, economic, environmental, and safety conditions in neighborhoods across the City and County. The Quality of Life Explorer has received national and international recognition for its innovative approach to analyzing the various factors that impact neighborhood vitality in Charlotte-Mecklenburg.

Environmental Stewardship

The City of Charlotte began implementation of its [Internal Operations Plan](#), a plan that was created in FY2014 to establish internal environmental efficiency goals for the next three calendar years. In FY2015, more than 35 of the City's internal environmental

operations plan goals were initiated with 11 of those goals being completed. These include conducting a citywide employee commuting survey, updating the city's greenhouse gas emissions inventory and obtaining approval for the creation of a 15-vehicle motor pool pilot.

Public Safety

The [Charlotte Mecklenburg Police Department](#) experienced improvements in several of its key public safety indicators including a 9.6 percent reduction in robberies, a 15.5 percent increase in the number of illegal guns seized, a 9.5 percent increase in animal adoptions, and a 78 percent clearance rate for homicide cases.

Community Engagement

Throughout FY2015, the City of Charlotte conducted numerous community engagement events to provide residents and businesses with information on City programs and services:

- 1,950 volunteers engaged in community improvement activities through the [Neighborhood Matching Grant Program](#).
- 250 citizens engaged through participation in [Neighborhood Board Retreats](#).
- 906 [Need-A-Speaker](#) resident engagements.
- 1,147 volunteers participating in community clean-ups through [Keep Charlotte Beautiful](#).
- 5,376 participants in the [Mayor's Youth Employment Program](#).



Focus Areas and Performance Measures

The City is committed to using performance measures to manage and address community needs. These are organized according to City Council’s five Focus Areas:



Community Safety

Charlotte will be one of America’s safest communities.



Economic Development & Global Competitiveness

Charlotte will strengthen its position as a city of prominence in the global marketplace by building upon its competitive advantages.



Environment

Charlotte will become a global leader in environmental sustainability preserving our natural resources while balancing growth with sound fiscal policy.



Housing and Neighborhood Development

Charlotte will sustain and create distinct and diverse neighborhoods for residents of all ages.



Transportation and Planning

Charlotte’s strong economy and attractive lifestyle will thrive due to our vibrant places, healthy neighborhoods and robust employment centers supported by strategic transportation investments.



CHARLOTTE.

MAYOR

Dan Clodfelter

MAYOR PRO TEM

Michael D. Barnes

CITY COUNCIL

At-Large

Claire Fallon

David L. Howard

Vi Lyles

District 1

Patsy Kinsey

District 2

Al Austin

District 3

LaWana Mayfield

District 4

Greg Phipps

District 5

John Autry

District 6

Kenny Smith

District 7

Ed Driggs

CITY MANAGER

Ron Carlee

For additional information, contact the City of Charlotte Office of Strategy & Budget.

citybudget.charmeck.org

704-336-2306



Performance Measures

1) Fire Education

Provide fire education program to 100 percent of CMS 3rd grade classrooms. **✓ 2015 status: 100%**

2) Reduce Crime and Loss of Life

Reduce crime in Part One Uniform Crime categories per 100,000 population by 3 percent below the prior year: homicide, rape, robbery, aggravated assault, burglary, larceny, vehicle theft and arson.

(Charlotte-Mecklenburg's part one offense crime rate decreased for six consecutive years from FY2009-FY2014. Despite having a slight increase in FY2015, the unified crime rate per 100,000 population is -1.6 percent below the three-year average for FY2012-FY2014.)

✗ 2015 status: +3.4%

3) Fire Prevention

Conduct 95 percent of fire code inspections within state mandated frequencies. **✓ 2015 status: 100%**

4) Develop a workforce that is reflective of the community's demographics

Implement recruitment strategies that attract a diverse police officer applicant pool of at least 40 percent women and minorities. **✓ 2015 status: 59.9%**

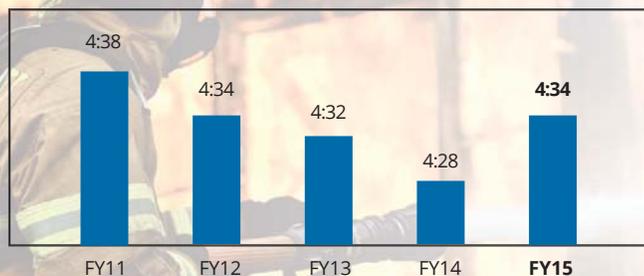
5) Fire Response Time

Arrive at scene of emergency within six minutes of receiving call at least 80 percent of the time. **✓ 2015 status: 84.5%**

6) Reduce the Number of Youth in the Criminal Justice System

Through CMPD's Juvenile Diversion Program, achieve a 70 percent or better success rate of program participants avoiding additional criminal charges while in the program. **✓ 2015 status: 95%**

AVERAGE FIRE RESPONSE TIMES (In Minutes)



PART ONE OFFENSE CRIME RATE (Per 100,000 Population)



Police Chief Kerr Putney talks with the community at a Cops & Barbers Town Hall meeting.



Economic Development & Global Competitiveness

Performance Measures

1) Continuous Improvement within the Permitting and Regulatory Environment

Complete land development permit submissions with an average of 2.5 or less reviews, reflecting process improvements and facilitating business development.

✓ 2015 status: **1.9**

2) Promote Foreign Direct Investment

Pursue at least five leads for foreign direct investment through the International Relations Office.

✓ 2015 status: **9**

3) Grow Small Businesses

Increase utilization of the City's business resources portal, CharlotteBusinessResources.com, by 5 percent to meet the needs of emerging and growing small businesses.

✓ 2015 status: **77%**

4) Hospitality Tax Revenue

Increase hospitality tax revenue by more than 3 percent.

✓ 2015 status: **11%**

5) Focus on Job and Tax Base Growth in Business Corridors

Create a global logistics center strategy for the area surrounding the Charlotte Douglas International Airport.

(Although this measure was not achieved in FY2015, a Request for Proposals was issued by Aviation for a consultant to conduct the necessary analysis for the global logistics center strategy. The reported result represents the percentage completion to-date.)

X 2015 status: **20%**



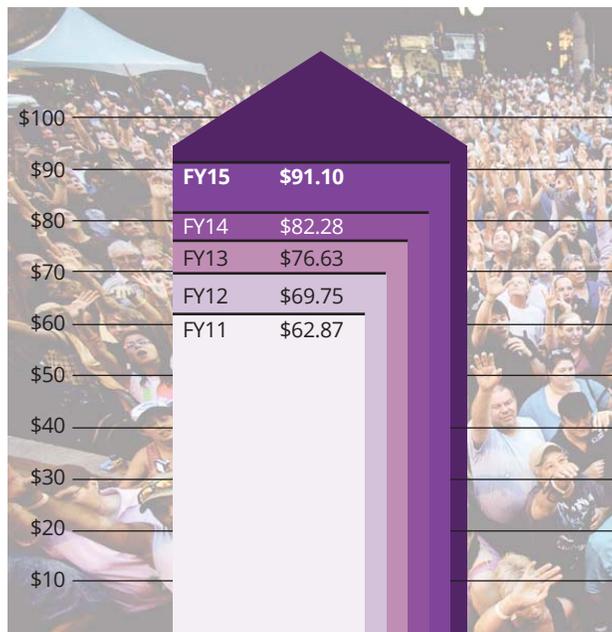
Above: In FY2015, the Charlotte Douglas International Airport opened the new hourly parking deck and consolidated rental car facility, creating 4,000 additional public parking spaces and improving the efficiency and customer-friendliness of the Airport's rental car facilities.

Below: View of Blue Line extension construction at East 11th Street.



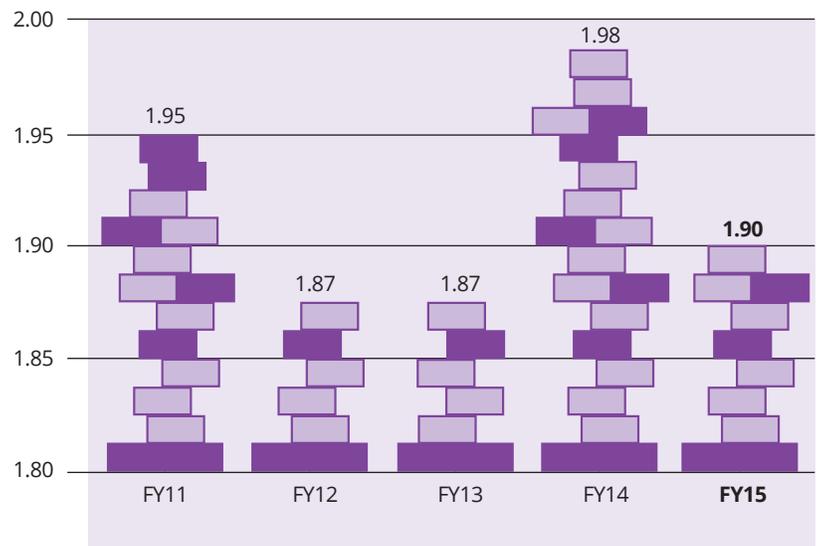
HOSPITALITY TAX REVENUE

(In Millions)



LAND DEVELOPMENT PERMIT REVIEWS

(Average Reviews Per Submission)





Performance Measures

1) Reduce Sanitary Sewer Overflows

Reduce sanitary sewer overflows to below 6.3 per 100 miles of sewer pipe.

✓ 2015 status: **4.1** (per 100 miles of sewer)

2) Litter Rating

Achieve 2.0 or lower on Keep America Beautiful litter rating on a scale of 1-4, where 1 is best score.

✓ 2015 status: **1.83**

3) Maintain Significant and Healthy Tree Canopy

Implement program to leverage public and private partnerships to plant the greatest number of trees possible toward the goal of a 50 percent tree canopy by 2050.

✓ 2015 status: **47 events with 4,257 trees planted & 7,500 seedlings distributed**

4) Improve efficiency of City facilities

Achieve Energy Star Use Intensity score of less than 67. (Energy Use Intensity measures the total energy consumed by a building in one year.)

✓ 2015 status: **64.3**

5) Achieve 100 Percent Regulatory Compliance

Meet all applicable requirements of the Safe Drinking Water Act and Clean Water Act. *(This measure was not achieved because a sample was not collected at an out-of-service tank. The site was later resampled and complied with all water quality standards.)*

✗ 2015 status: **99.9%**

6) Reduce Residential Waste

Reduce pounds landfilled per curbside residential unit compared to prior year to a total of 1,669.2 pounds.

✓ 2015 status: **1,666.4**



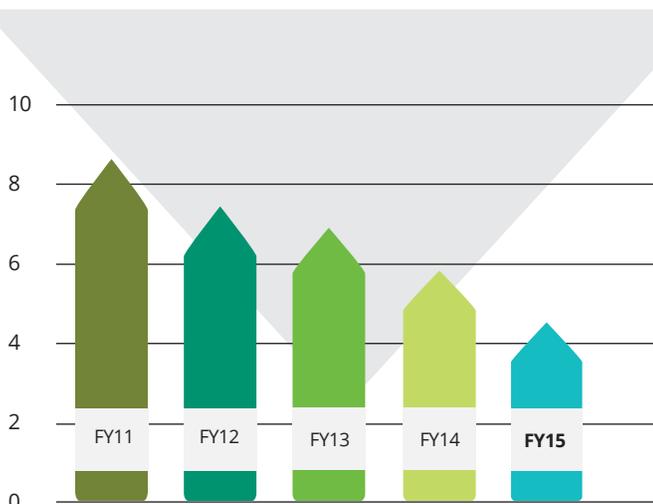
BigBelly solar waste stations encourage recycling and reduce litter in Uptown and South End Charlotte with on-site compaction and sensors that detect when they are full reducing the frequency of collection by 72 percent.



Sustainability Coordinator Darcy Everett discussing the city's environmental initiatives at the 2015 Earth Day Festival.

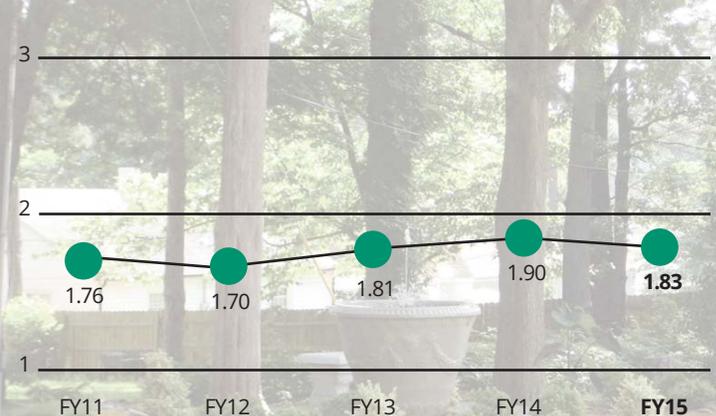
SANITARY SEWER OVERFLOWS

(Overflows per 100 miles of pipe)



KEEP AMERICA BEAUTIFUL LITTER INDEX RATING

(1.0 is best score)





Housing and Neighborhood Development

Performance Measures

1) Release Updated Quality of Life Explorer

Release updated Quality of Life Explorer by June 30, 2015.

✓ 2015 status: 100%

2) Promote Diverse Housing Options Throughout the Community

Allocate Housing Trust funds to at least two developments consistent with Housing Policy goal.

✓ 2015 status: 16

3) Develop Policies that Assist with Increasing the Supply of Affordable Housing

Attain at least five developer inquiries and approvals for new voluntary single and multi-family density programs.

✓ 2015 status: 7 inquiries

4) Healthy and Vibrant Neighborhoods

Support 30 neighborhood organizations in developing and implementing neighborhood improvement goals through Neighborhood Board Retreats.

✓ 2015 status: 40

5) Support Programs that End and Prevent Homelessness

Begin implementation of the "A New Way Home" rental subsidy pilot by providing housing and supportive services to at least ten families. (A request for proposals, or RFP, was issued in FY2015 to select agencies equipped to provide rental assistance and supportive services for families receiving rental subsidies through the "A Way Home" pilot. Two organizations were selected through the RFP process and it is anticipated that participants will be selected for the pilot in FY2016).

x 2015 status: 0 families

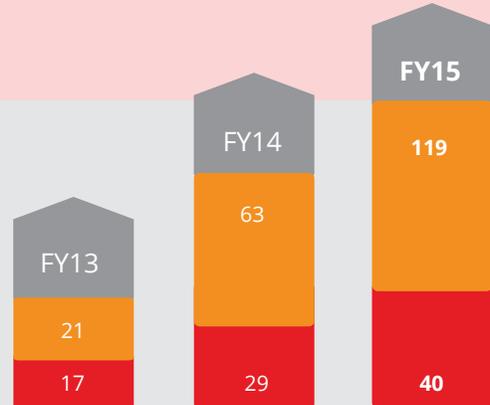


The historic Mecklenburg Mill now provides 48 units of quality affordable housing along the Blue Line Extension. The restoration of The Lofts at NoDA Mills was accomplished with a combination of state and federal funding.

NEIGHBORHOOD ORGANIZATIONS ATTENDING BOARD RETREATS

Retreat Applicants (Red) Retreat Attendees Selected (Orange)

The first Neighborhood Board Retreat was held in FY2013.



Homes located in the Brightwalk Community, which is a mixed-income development located in Charlotte's North End that is the result of public/private partnership.



Performance Measures

1) Aviation Cost per Passenger

Maintain status as having the lowest cost per enplanement among American Airlines hub cities. **✓ 2015 status: \$1.56**

2) CATS System Performance

Maintain an average system on-time performance of greater than or equal to 84 percent for bus and greater than or equal to 98.5 percent for rail. **✓ 2015 status: 87.3%, 99.6% rail**

3) Bikeways and Sidewalks

Increase bikeways and sidewalks by at least 10 miles each annually. **✓ 2015 status: 10.24 bike miles, 19.31 sidewalk miles**

4) Vehicle Accidents

Decrease percent of motor vehicle accidents to fewer than two vehicle accidents per million vehicles entering an intersection. **✓ 2015 status: 1 accident per .81 million entering vehicles**

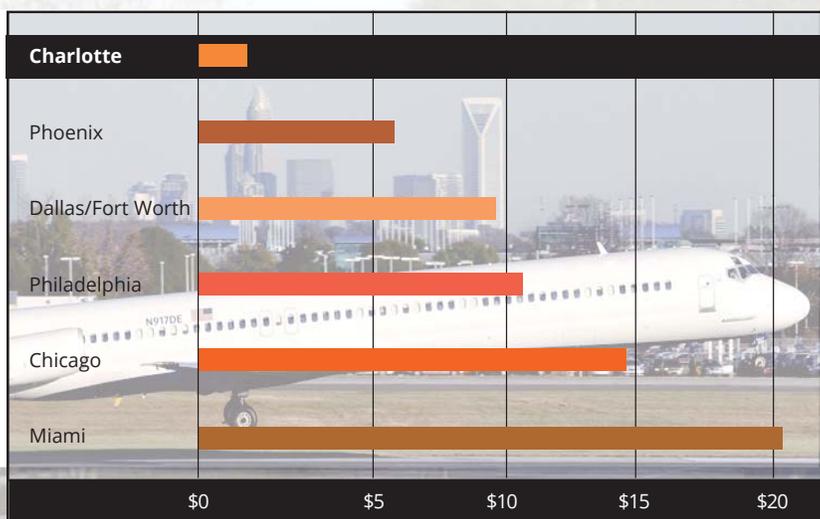
5) Walkability and Bikeability

Improve Charlotte's walk score relative to peer cities. **✗ 2015 status: Walk Score Ranking of 50**

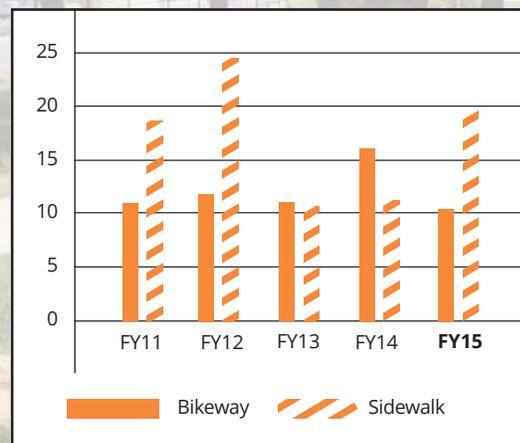


Running 1.5 miles from the Uptown CATS Transit Center to the Elizabeth Area, the Lynx Gold Line streetcar opened June 2015.

AIRLINE COST PER ENPLANEMENT



INCREASE IN MILES OF BIKEWAYS AND SIDEWALKS



VALUE OF CITY SERVICES

On a typical day in Charlotte, examples of City services that residents depend on include:

255 firefighters on duty at 42 fire stations responding to more than 260 calls for service

755 signalized intersections operating over 2,400 miles of streets

2,600 officer-initiated calls for service

4,400 service calls received by CharMeck 311

42,500 households receiving trash pick up

80,630,000 gallons of wastewater collected and treated daily

102,570,000 gallons of drinking water collected and treated daily

CharMeck 311 representatives providing service to citizens at the call center.

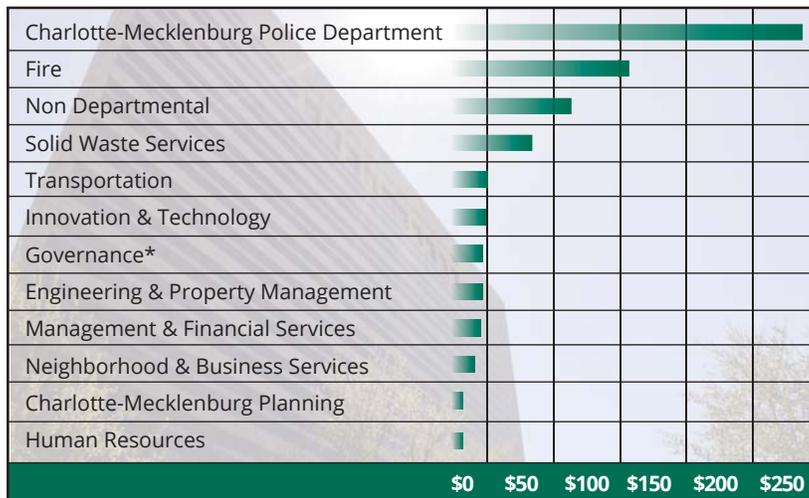
FY2015 CITY OF CHARLOTTE TAXES & FEES

City of Charlotte taxes & fees (50th percentile home value)	Prior Year FY2014	FY2015 Budget	\$ Change	% Change
Property taxes on \$151,300 home*	\$709.14	\$709.14	\$0.00	0.0%
Solid Waste Fee (residential)	\$47.00	\$47.00	\$0.00	0.0%
Water & Sewer (avg. user rate)	\$662.04	\$682.80	\$20.76	3.1%
Storm Water (avg. user rate)	\$94.68	\$97.56	\$2.88	3.0%
Total Annual	\$1,512.86	\$1,536.50	\$23.64	1.6%
Total Monthly	\$126.07	\$128.04	\$1.97	1.6%

*The FY2015 City Property Tax Rate was 46.87¢ per \$100 valuation

FY2015 GENERAL FUND OPERATING EXPENDITURES

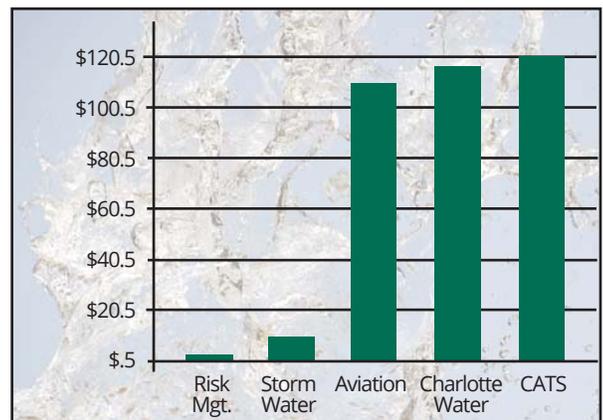
(In Millions)



*Governance includes the City Attorney, City Clerk, City Manager, and Mayor and City Council.

FY2015 ENTERPRISE AND INTERNAL SERVICE FUND OPERATING EXPENDITURES

(In Millions)



**Career Readiness Training Certification Schedule
2015 - 2016**

Council District (Student Catchment)	High School	Address	City	Training Date	Training Time	Graduation Time
1/5/6/7**	East Mecklenburg	6800 Monroe Rd.	Charlotte	9/30/2015	7:15 am - 2:15 pm	2:00 PM
1/2/3/4**	West Charlotte	2219 Senior Dr.	Charlotte	10/1/2015	7:15 am - 2:15 pm	2:00 PM
1/3/6**	Harding University	2001 Alleghany St.	Charlotte	10/2/2015	7:15 am - 2:15 pm	2:00 PM
All**	Phillip O. Berry	1430 Alleghany St.	Charlotte	10/7/2015	7:15 am - 2:15 pm	2:00 PM
1/4/5	Garinger	1100 Eastway Dr.	Charlotte	10/12/2015	7:15 am - 2:15 pm	2:00 PM
5/6/7*	Butler	1810 Matthews-Mint Hill Rd.	Matthews	10/13/2015	7:15 am - 2:15 pm	2:00 PM
All**	Cato Middle College/ Middle College High at Levine Campus	8120 Grier Rd. Suite 171-B (Cato Middle College)	Charlotte	11/3/2015	12:30 pm - 4:15 pm	4:00 PM
2/5*	Independence	1967 Patriot Dr.	Charlotte	11/4/2015	7:15 am - 2:15 pm	2:00 PM
2*	Hopewell	11530 Beatties Ford Rd.	Huntersville	11/5/2015	7:15 am - 2:15 pm	2:00 PM
3/6/7**	South Mecklenburg	8900 Park Rd.	Charlotte	11/9/2015	7:15 am - 2:15 pm	2:00 PM
1/2/4/5	Vance	7600 IBM Dr.	Charlotte	11/10/2015	7:15 am - 2:15 pm	2:00 PM
2/4* **	North Mecklenburg	11201 Old Statesville Rd.	Huntersville	11/12/2015	7:15 am - 2:15 pm	2:00 PM
1/4/5*	Rocky River	10505 Clear Creek Commerce Dr.	Charlotte	11/13/2015	7:15 am - 2:15 pm	2:00 PM
All	Supplemental Training Day at Charlotte Mecklenburg Library - Main (all schools)	310 N. Tryon St.	Charlotte	11/14/2015	10 am - 4:30 pm	4:00 PM
3	Olympic	4301 Sandy Porter Rd.	Charlotte	11/16/2015	7:15 am - 2:15 pm	2:00 PM
7	Ardrey Kell	10220 Ardrey Kell Rd.	Charlotte	11/17/2015	7:15 am - 2:15 pm	2:00 PM
2/4	Mallard Creek	3825 Johnston Oehler Rd.	Charlotte	11/18/2015	7:15 am - 2:15 pm	2:00 PM

*Campus sits outside City limits but student catchment area includes Council District(s) indicated

**Campus maintains magnet program or other specialty option, pulling from broader areas of the City/County



September 25, 2015

CNIP community meeting updates



Enthusiasm remained high on the westside when the W. Trade/Rozelles Ferry Road Comprehensive Neighborhood Improvement Program (CNIP) team hosted 74 stakeholders at a community meeting last month at Johnson C. Smith's Mosaic Village.

The team presented a list of 115 potential candidate projects identified at previous community meetings, highlighting projects such as the West 4th St. extension. The team also explained the multi-phase vetting process that will allow the most viable projects to emerge. The process includes:

- Removal of projects that duplicate existing work
- Removal of projects ineligible for CNIP bond money
- Selection of projects that offer one or more of the following features:
 - Near shovel-ready and therefore could be completed quickly
 - Ability to leverage current opportunities and partnerships
 - A mix of small and large cost projects
 - Opportunity to serve as a catalyst project to attract partner funding

The meeting then focused on 23 projects that rated favorably based on the CNIP goals and neighborhood criteria. After the presentation, stakeholders were divided into breakout sessions where they were able to ask questions and provide feedback on the community's priorities.

Although funding is not anticipated for implementation of all projects at this time, the input from the workshop will be shared with the CNIP Coordination Team as they continue to identify key projects to move forward.

LYNX Silver Line

In 2011 the Metropolitan Transit Commission directed CATS to prepare for a LYNX Silver Line/Southeast Corridor Transit Study. The project officially began in February 2015. CATS, city staff, NCDOT, and the Town of Matthews have been developing a proactive public engagement plan, analyzing existing conditions and setting goals for the project. The study, which is anticipated to be complete by the end of June 2016, has three overarching goals:

- Define a fixed rail guideway alignment that serves future transportation needs and promotes the land use plans/policies of both the Independence Boulevard Area Plan and the Town of Matthews.
- Provide an interim transit strategy that utilizes the future managed lanes proposed on Independence Boulevard.
- Coordinate with land development strategies to protect and preserve the fixed guideway alignment.

During the month of August, the team conducted three public meetings where residents participated in interactive exercises and map activities. The purpose of the activities was to gain a better understanding of how the public feels about various types of rail and bus operating scenarios, opportunities, and constraints within the Southeast Corridor.

Full survey data and comments are currently being summarized; however, here are a few highlights of the information thus far:

- A total of 189 residents attended the three workshops.
- The majority at the Matthews and Uptown Library workshops agreed that rail in the corridor should have an efficient and reliable travel time, even if significant property must be acquired for a dedicated right-of-way. A slight majority of people attending the Midwood International and Cultural Center agreed with this.

Work during the month August also included continued preparation of bus route recommendations for the Southeast Corridor as well as the Independence Boulevard express lanes.

Data team scores big at Charlotte Business Journal's CIO Awards

In August, Jan Whitesell accepted the Best Big Data Project award at the Charlotte Business Journal's annual CIO of the Year awards. The award was for her team's work on the city's CIP.

When the CIP process started, the city didn't have funds to onboard a technology to collect data and analyze projects within the plan. The solution was to use existing tools and highly skilled staff to create a comprehensive technical platform. The team used over 200 datasets in ESRI's Model Builder and technology with ArcMap and Tableau to create eight databases, an ArcGIS online map service, eight storybook dashboards, a Virtual Charlotte data browsing application and an [online portal](#).



The team utilized these technologies to report existing conditions in each strategy area, ensuring that all consultants had a common baseline of data from which to start their analysis. It also relieved city GIS analysts from having to provide multiple copies of very large datasets to all of the consultants.

The data allowed strategy teams to assess where opportunities and challenges existed in project areas. It also helped departments coordinate upcoming projects within the same area and hold joint public meetings where all projects were represented.

By using in-house data, staff and technology tools, the project saved an estimated \$250,000 in data gathering and analysis, and reporting costs from outside contracts.

The project team included: Andrew Bowen (N&BS), Steven Castongia (CDOT), Rebecca Hefner (N&BS), Jason Lawrence (CATS), Evan Lowry (Planning), Katie Mayr (CDOT), Sou Moua (E&PM), Lori Quinn (I&T), Thomas Ricard (Storm Water Services), Keri Shearer (I&T), Jan Whitesell (Planning – Project Lead) and Samantha Willis (Storm Water Services).

Cross Charlotte Trail in the news

Over the summer, members of the Cross Charlotte Trail team were interviewed on [WFAE Charlotte Talks](#) to discuss the trail progress, and the transportation and economic development benefits of trail-oriented development.

Over the next several months, city staff will work with the consultant team to finalize the Cross Charlotte Trail Master Plan and alignment study. City staff will bring several agenda items to Council in September and October with regard to the master plan and coordination. These items will include planning and design contracts for new segments of trail, two interlocal agreements with Mecklenburg County and developer agreements with at least two developers regarding construction of parts of the trail.

[Upcoming Meetings](#)

Prosperity Village CNIP

CrossWay Community Church (6400 Prosperity Church Road)

Sept. 29, 6-8 p.m.

Sunset Beatties Ford CNIP

Friendship Missionary Church (3400 Beatties Ford Rd)

Sept. 29, 10 a.m. – noon; 1-3 p.m.; 4-6 p.m.

Central/Albemarle/Shamrock CNIP

Midwood International & Cultural Center (1817 Central Avenue)

Oct. 1, Drop-in 6 – 8 p.m.

BLE Meetings

The Oasis Shriners (604 Doug Mayes Place)

Oct. 13, 6 p.m. – 7:30 p.m.

Charlotte Mecklenburg Public Library, Main (310 N. Tryon Street)

Oct. 14, Drop-In Only 11:30 a.m. – 1:30 p.m.

Sugaw Creek Church (101 W Sugar Creek Road)

Oct. 15, 6 p.m. – 7:30 p.m.

For more CIP and CNIP information, visit [Charlotte Future](#).



CHARLOTTE™

**CITY MANAGER'S OFFICE
M E M O R A N D U M**

September 25, 2015

TO: Ron Carlee, City Manager
Ron Kimble, Deputy City Manager

FROM: Dana Fenton, Intergovernmental Relations Manager

SUBJECT: **2015 State Legislative Report #32**

Major Issues

Pursuant to the adjournment resolution passed by the Senate and sent to the House, House and Senate will adjourn Tuesday, September 29 ([SR 721 – Apodaca](#)) and convene the 2016 short session on April 25, 2016

NC Competes Act ([HB 117 – Martin](#))

- Passed House and Senate and sent to Governor for signature
- HB 117 continues Job Development Investment Grant and One NC economic development programs
- HB 117 fully exempts aviation fuel from sales taxes beginning January 1, 2016, which impacts General Fund to the tune of \$950,000 annually and the Transit Fund \$750,000 annually

Connect NC Bond Act of 2015 ([HB 943 – Rules](#))

- Passed Senate and sent to House for concurrence, with first of two concurrence votes scheduled in the House for Monday, September 28
- \$2 billion bond issue for projects in the UNC and community college systems, parks, national guard, agriculture and public safety areas
- Vote of the people would be at the same time as the 2016 Presidential Primary, which is set for Tuesday, March 15, 2016

2016 Presidential Primary ([HB 373 - Riddell](#))

- Conference report passed House and Senate and sent to Governor for signature
- Presidential Primary scheduled for Tuesday, March 15, 2016

Medicaid Reform Act of 2015 ([HB 372 – Dollar](#))

- Conference report approved by House and Senate and sent to Governor for signature

Regulatory Reform Act of 2015 ([HB 765 – McElraft](#))

- House and Senate will consider the conference report on Monday, September 28

Protect NC Workers Act ([HB 318 – Cleveland](#))

- Section 11 of HB 318 prohibits local governments from issuing local identification cards, unless authorized by General Assembly
- Section 15 prohibits cities and counties from adopting so-called sanctuary city policies, which is in reaction to events in San Francisco; Charlotte was identified on the floor of the Senate by Senator Jerry Tillman (R-Randolph) as one of several sanctuary cities in the State
- Final reading by the Senate will be Monday, September 28
- If HB 318 passes its final reading in the Senate, it will be sent back to the House for concurrence

Various Changes to the Revenue Laws ([SB 605 – Rucho](#))

- Proposed committee substitute presented to and considered by House Finance Committee included contents of HB 903 that would have authorized cities to adopt, by resolution, ¼ percent sales tax within their corporate limits and all of the revenue would have remained with the City (Part VII)
- HB 903 was sponsored by Representative Jason Saine, one of two Senior Chairman of the House Finance Committee
- Part VII was removed from the bill by the committee as members did not want to open the door to varying sales tax rates within counties

Environmental & Planning

Outdoor Advertising ([HB 304 – Hager, Collins, J. Bell and Hanes](#) / [SB 320 – Brown, Rabon and Tarte](#)) preempts local authority with respect to the location, height and size of relocated signs and conversion to changeable message signs and replaces these with statewide standards. The legislation appears to open the door to relocating outdoor advertising from industrially zoned areas to commercially zoned areas and allowing such signs to be higher and larger than those allowed under existing local ordinance. HB 304 received serial referrals to House Commerce and Finance Committees, and SB 320 received serial referrals to Senate Commerce and Finance Committees.

City Requested Legislation

Stormwater Management ([HB 141 – Jeter, Cotham, Cunningham, Bradford, Bryan, Carney, Earle and R. Moore](#)) authorizes municipalities in Mecklenburg, Wake, Durham, Forsyth and Guilford Counties to utilize the statutory authority extended to Mecklenburg and Wake Counties in 2014 to implement flood damage reduction techniques that result in improvements to private property. HB 141 passed the House and was referred to Senate Rules.